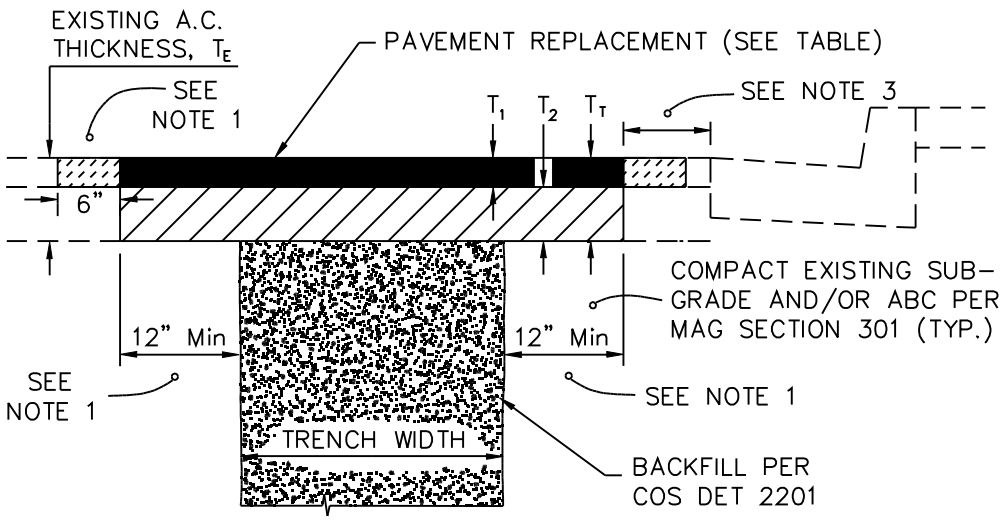
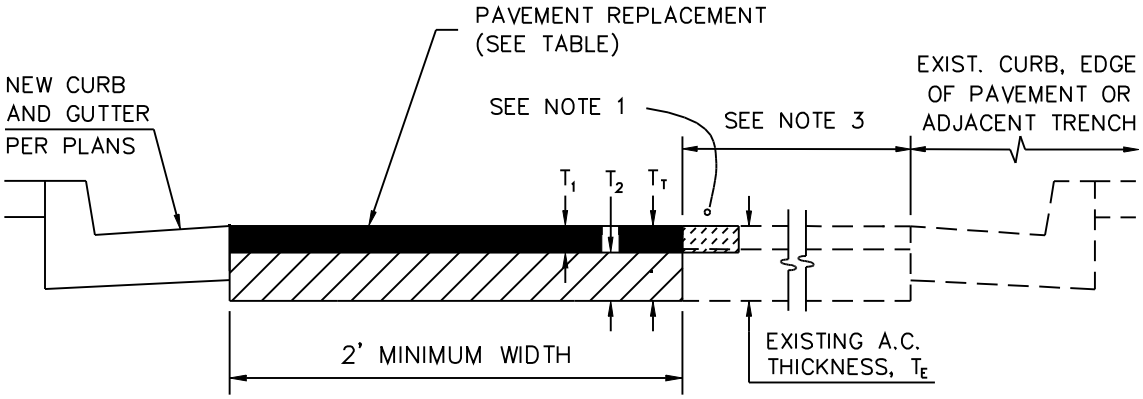


PAVEMENT REPLACEMENT FOR TRENCHES (T-TOP)



PAVEMENT REPLACEMENT

EXISTING PAVEMENT THICKNESS, T_E	AC PAVEMENT REPLACEMENT TABLE		
	AC SINGLE COURSE OR SURFACE COURSE, T_1	AC BASE COURSE, T_2	TOTAL THICKNESS, T_T
$T_E \leq 3"$	3" MINIMUM	NONE	3" MINIMUM
$T_E > 3"$	2" MINIMUM	2" MINIMUM	T_E (MATCH EXIST)

PAVEMENT REPLACEMENT NOTES

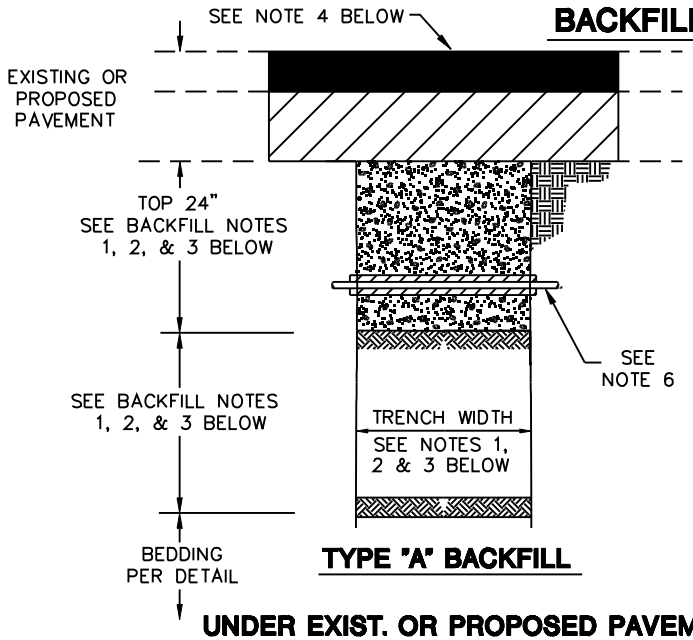
1. "T"-TOP REQUIRED FOR ALL TRENCHES. A.C. SURFACE COURSE REPLACEMENT TO BE MILLED DOUBLE "T" CONFIGURATION AS SPECIFIED BELOW FOR PAVEMENTS 4" AND THICKER.
   a. FOR PAVEMENT 4 YEARS AND OLDER: INITIAL A.C. REMOVAL TO BE THE MINIMUM WIDTH REQUIRED FOR PROPER TRENCH COMPACTION. SAWCUT & REMOVE 12" OF A.C. MINIMUM ON EACH SIDE OF THE TRENCH FOR THE "T"-TOP AFTER THE BACKFILL MATERIAL IS PLACED. PAVEMENTS 4" AND THICKER, MILL AND REMOVE THE TOP 2" OF THE SURFACE COURSE A MINIMUM OF 6" ON EACH SIDE OF THE T-TOP PRIOR TO PLACEMENT OF THE FINAL SURFACE COURSE LIFT.
   b. FOR NEW AND OVERLAYED PAVEMENT LESS THAN 4 YEARS OLD AND WHEN ALLOWED UNDER THE PROVISIONS OF SCOTTSDALE REVISED CODE SECTIONS 47-79 AND ALL PAVEMENTS WITH RUBBERIZED SURFACE COURSES: INITIAL A.C. REMOVAL TO BE THE MINIMUM WIDTH REQUIRED FOR PROPER TRENCH COMPACTION. SAWCUT & REMOVE 12" OF A.C. MINIMUM ON EACH SIDE OF THE TRENCH FOR THE "T"-TOP AFTER THE BACKFILL MATERIAL IS PLACED. PAVEMENTS 4" AND THICKER, MILL AND REMOVE THE TOP 2" OF THE SURFACE COURSE EQUALLY ON BOTH SIDES OF THE TRENCH TO A MINIMUM TOTAL WIDTH OF 10 FEET. FOR PAVEMENTS LESS THAN 4" THICK SAWCUT, REMOVE AND REPLACE THE ENTIRE PAVEMENT SURFACE TO A MINIMUM TOTAL WIDTH OF 10 FEET, AS DIRECTED BY THE ENGINEER.
   c. FOR DEEP PAVEMENT STRUCTURES REQUIRING TWO OR MORE PAVEMENT BASE LIFTS: INITIAL A.C. REMOVAL TO BE THE MINIMUM WIDTH REQUIRED FOR PROPER TRENCH COMPACTION. SAWCUT, REMOVE AND REPLACE A.C. ON BOTH SIDES OF THE TRENCH AS NECESSARY TO ACCOMODATE A RIDE ON TYPE VIBRATORY ROLLER COMPACTOR FOR PLACEMENT OF THE A.C. BASE COURSE LIFTS, MATCH EXISTING A.C. DEPTH. MILL AND REMOVE THE TOP 2" OF THE SURFACE COURSE EQUALLY ON BOTH SIDES OF THE TRENCH TO A MINIMUM TOTAL WIDTH OF 10 FEET.
2. ASPHALT CONCRETE SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF MAG SECTION 321.
3. IF PAVEMENT REMNANT IS LESS THAN 36", REMOVE AND REPLACE PAVEMENT AS PER THIS DETAIL.
4. AGGREGATE BASE COURSE PER MAG SECTION 702 SHALL BE PROVIDED TO MATCH EXISTING ABC THICKNESS IN ADJACENT ROADWAY.
5. REFER TO COS SUPPLEMENTAL SPECIFICATIONS, SECTION 336.2.4 FOR PAVEMENT SMOOTHNESS REQUIREMENTS.

REVISED 7/15/03

DETAIL NO. 2200	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	PAVEMENT REPLACEMENT	DETAIL NO. 2200

REVISED 3/01/06

BACKFILL DETAILS

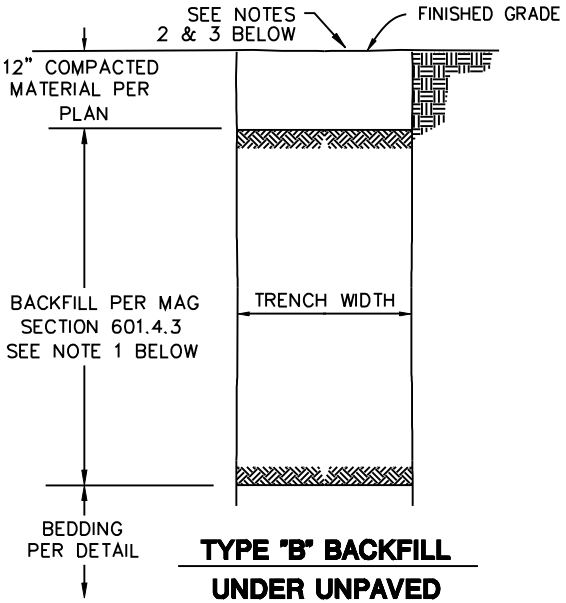


TYPE "A" BACKFILL

UNDER EXIST. OR PROPOSED PAVEMENT OR EXIST. SURFACE TREATMENT

BACKFILL NOTES

1. TRENCHES LESS THAN 24" IN WIDTH – 1/2 SACK CSLM, MAG 728, FULL DEPTH OF BACKFILL BOTTOM OF ABC LAYER IN PAVEMENT STRUCTURE OR 6" BELOW PAVEMENT IN FULL DEPTH PAVEMENT STRUCTURES. CONSTRUCT PAVEMENT STRUCTURE TO MATCH EXISTING AND IN ACCORDANCE WITH COS DETAIL 2200.
2. TRENCHES 24" TO 6' IN WIDTH – 1/2 SACK CSLM AS DESCRIBED IN NOTE 1 ABOVE WITHIN THE TOP 24" OF THE TRENCH; MAG 601.4.3 FOR BALANCE OF BACKFILL.
3. TRENCHES OVER 6' IN WIDTH – MAG 601.4.3 FULL DEPTH OF BACKFILL.
4. TREAT ENTIRE DISTURBED SURFACE OF UNPAVED ALLEYS WITH LIGNIN–BASED DUST PALLATIVE, MAG 792, 1:1 DILUTION RATIO, 0.50 GAL/SY APPLICATION RATE.
5. CSLM SHALL NOT BE USED FOR WATER OR SEWER PIPE BEDDING. SEE BEDDING DETAIL.
6. EXPOSED COPPER OR POLYETHYLENE WATER PIPES IN SIZES 3/4" TO 2" SHALL BE WRAPPED WITH 3/4" WIDE BLACK INSULATION BEFORE PLACING CSLM.
7. RECYCLED ASPHALT SHALL NOT BE USED FOR BACKFILL.



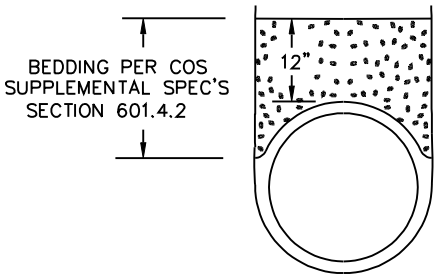
TYPE "B" BACKFILL

UNDER UNPAVED ROADWAY, PARKING LOT AND VACANT LAND

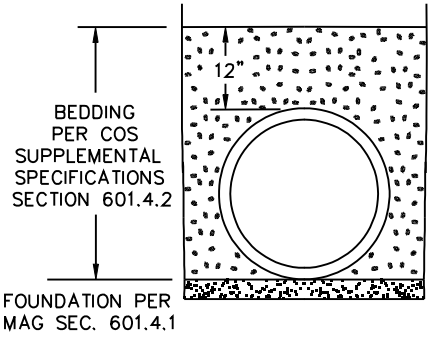
BACKFILL NOTES

1. MAG 601.4.3 FULL DEPTH OF BACKFILL
2. ENTIRE DISTURBED EXISTING SURFACE TO BE RESTORED WITH A LIKE MATERIAL
3. TREAT ENTIRE DISTURBED SURFACE WITH LIGNIN–BASED DUST PALLATIVE, MAG 792, 1:1 DILUTION RATIO, 0.50 GAL/SY APPLICATION RATE.
4. RECYCLED ASPHALT SHALL NOT BE USED FOR BACKFILL.

BEDDING DETAILS



BEDDING DETAIL
CAST-IN-PLACE PIPE



BEDDING DETAIL
ALL OTHER PIPE

NOTES:

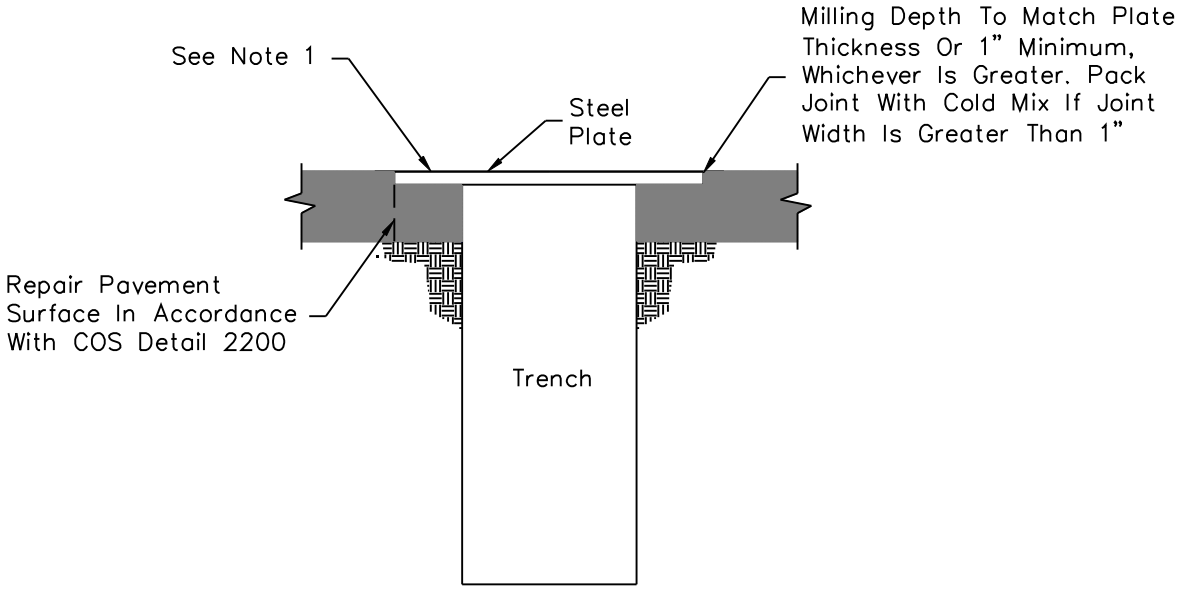
1. FOR HDPE PIPE – SEE COS SUPPLEMENTAL SPECIFICATIONS SECTION 603.4.2
2. RECYCLED ASPHALT SHALL NOT BE USED FOR BEDDING OR FOUNDATION MATERIAL.

DETAIL NO.		APPROVED BY:		DETAIL NO.
2201	City of Scottsdale Standard Details	Scottsdale Standards & Specifications Committee	TRENCH BEDDING & BACKFILL	2201

REVISED: 4/25/02

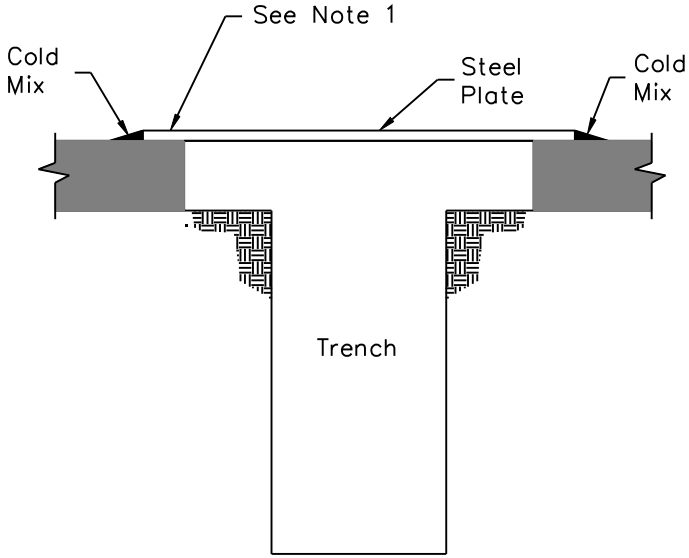
NOTES:

1. The contractor shall provide adequate overlap of plate on asphalt to assure no slippage of plate and no collapsing of trench.
2. "Posted Speed" does not include temporary construction signing.



TYPE "A" PLATING

CITY POSTED SPEEDS OF
35 MPH AND GREATER
OR BUS & TRUCK ROUTE

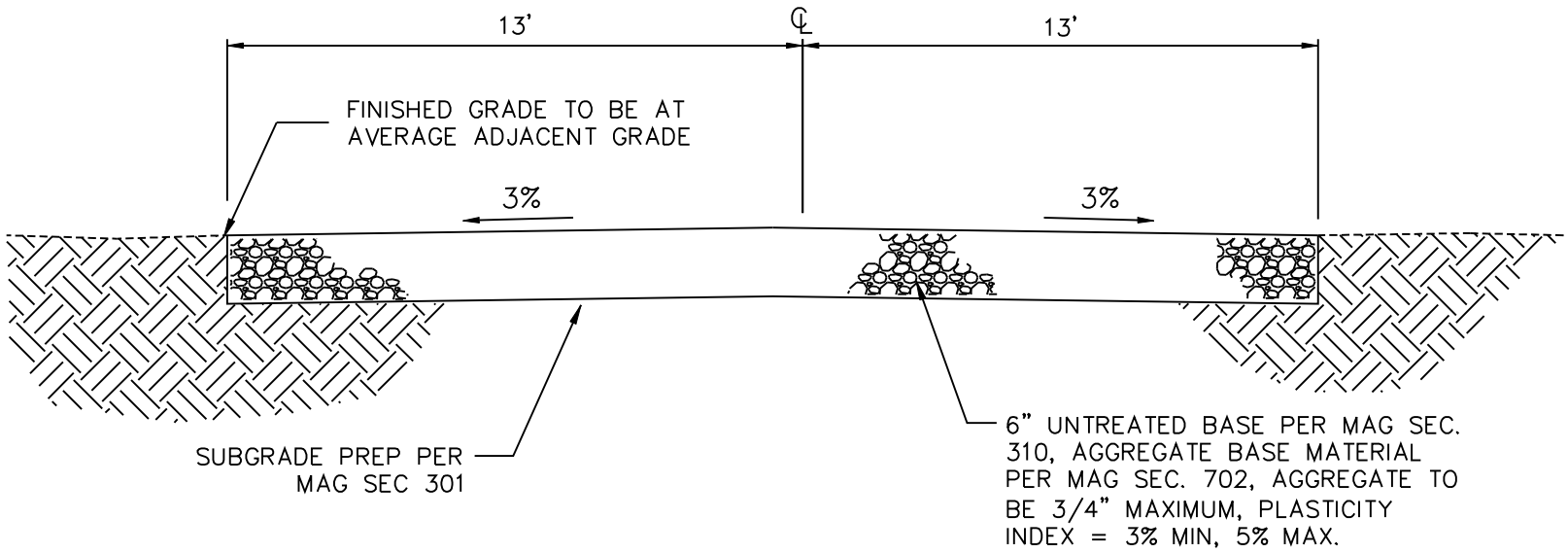


TYPE "B" PLATING

CITY POSTED SPEEDS
UNDER 35 MPH

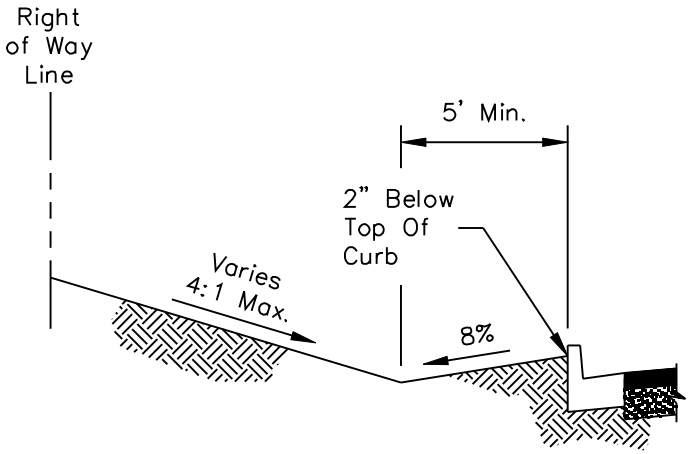
DETAIL NO. 2202	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	TRENCH PLATING	DETAIL NO. 2202

REVISED: 3/28/02

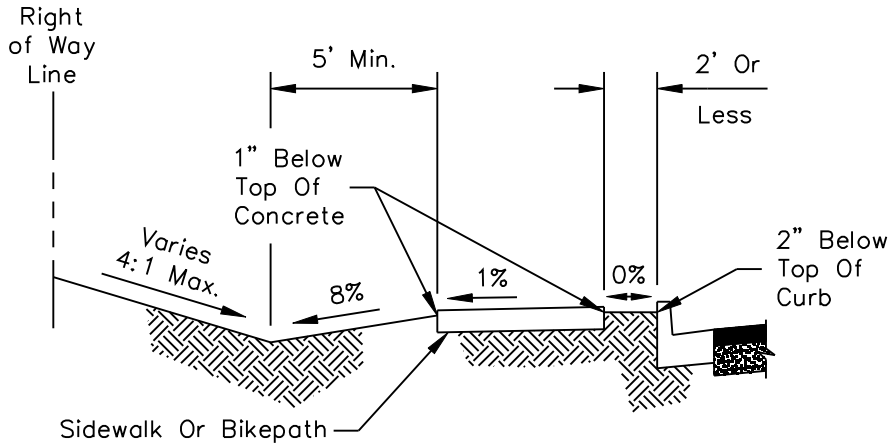


UNPAVED ROAD DETAIL

DETAIL NO. 2207	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	RESIDENTIAL UNPAVED ROAD	DETAIL NO. 2207



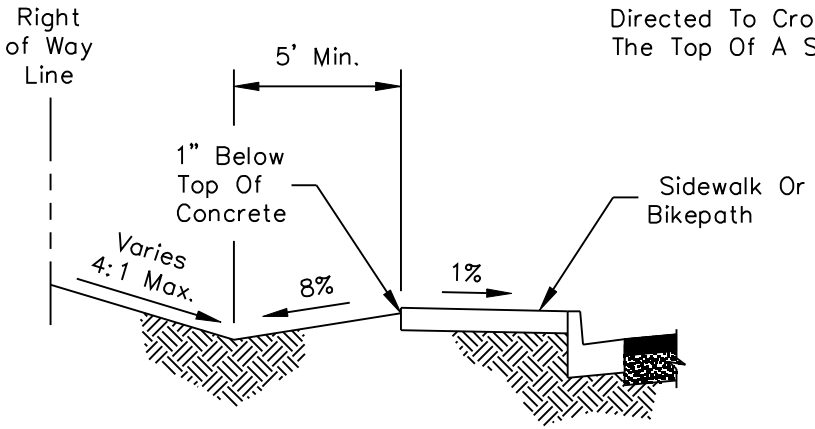
NO SIDEWALK



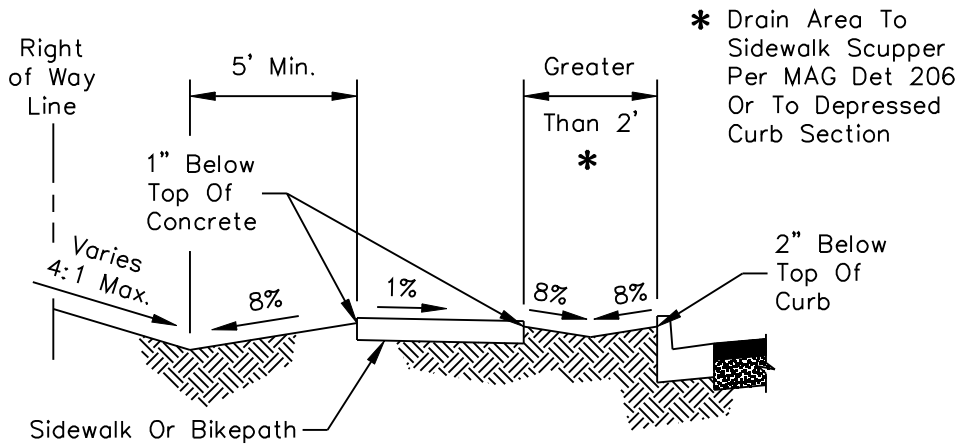
SIDEWALK LESS THAN 2' BEHIND CURB

NOTE:

Runoff Shall Not Be Directed To Cross Over The Top Of A Sidewalk.



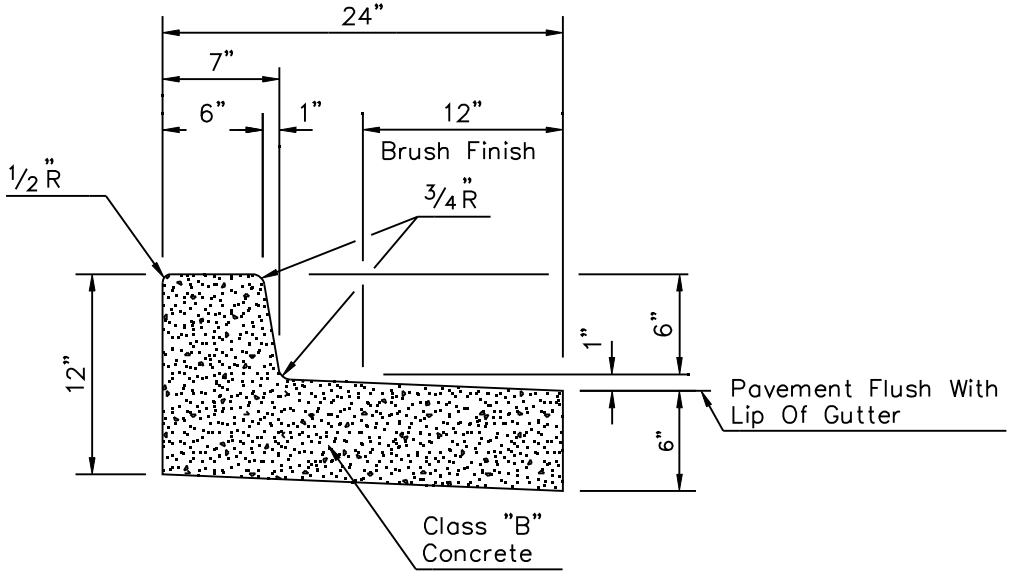
SIDEWALK AT BACK OF CURB



SIDEWALK MORE THAN 2' BEHIND CURB

DETAIL NO. 2210	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	# GRADING BEHIND THE CURB	DETAIL NO. 2210

REVISED 4/7/00

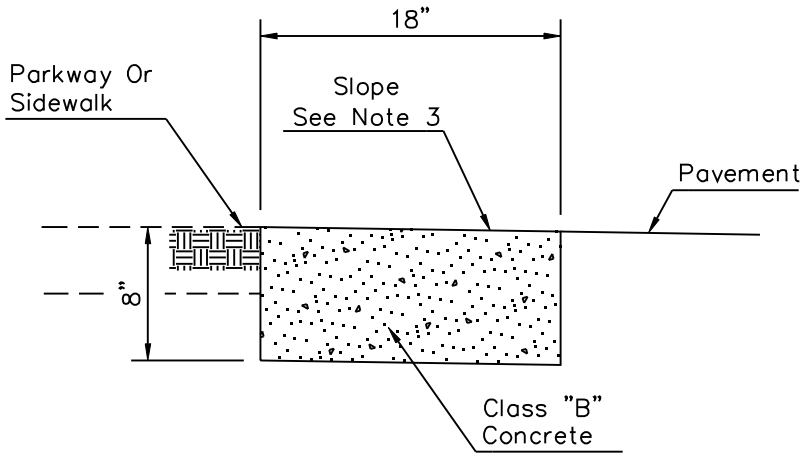


VERTICAL CURB & GUTTER WITH DEPRESSED LIP

TYPE "A"

NOTES

1. All exposed surfaces to be trowel finished except as shown. See M.A.G. Section 340.
2. Contraction joint spacing 10' maximum.
3. Construct curb and install 1/2" mastic expansion joints, A.S.T.M. D-1751, per M.A.G. Sec. 340 & 729 and COS Sec. 340.
4. Colored concrete shall be colored integrally.



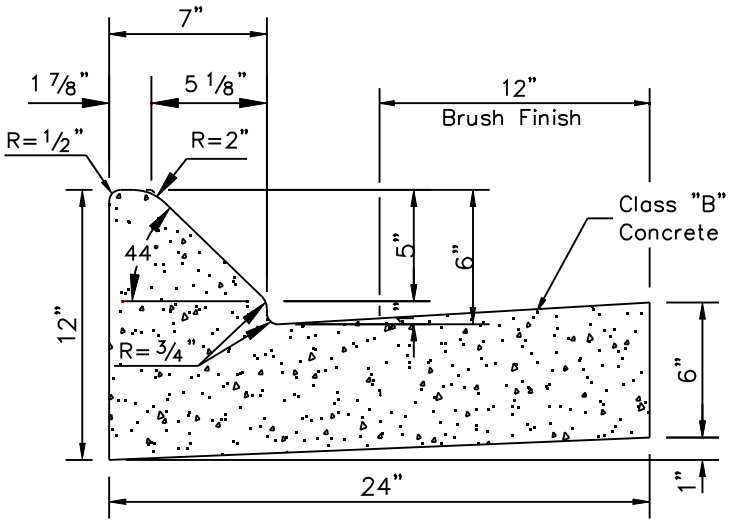
RIBBON CURB

TYPE "B"

NOTES

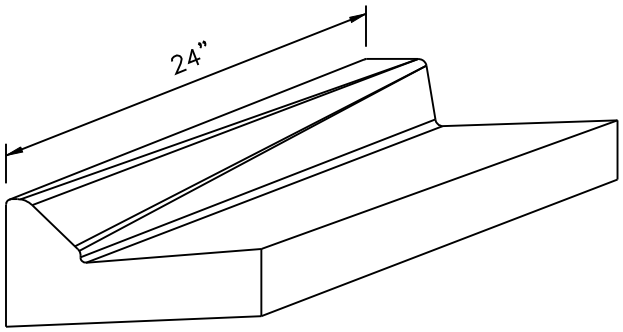
1. Construct curb and install 1/2" mastic expansion joints, A.S.T.M. D-1751, per M.A.G. Sec. 340 & 729 and COS Sec 340.
2. Broom finish all surfaces.
3. Ribbon curb may slope towards pavement or parkway. Match cross slope of road unless indicated otherwise on plans.
4. Contraction joint spacing 10' maximum.
5. Colored concrete shall be colored integrally.

DETAIL NO. 2220	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	CURB AND GUTTER - TYPES A & B	DETAIL NO. 2220

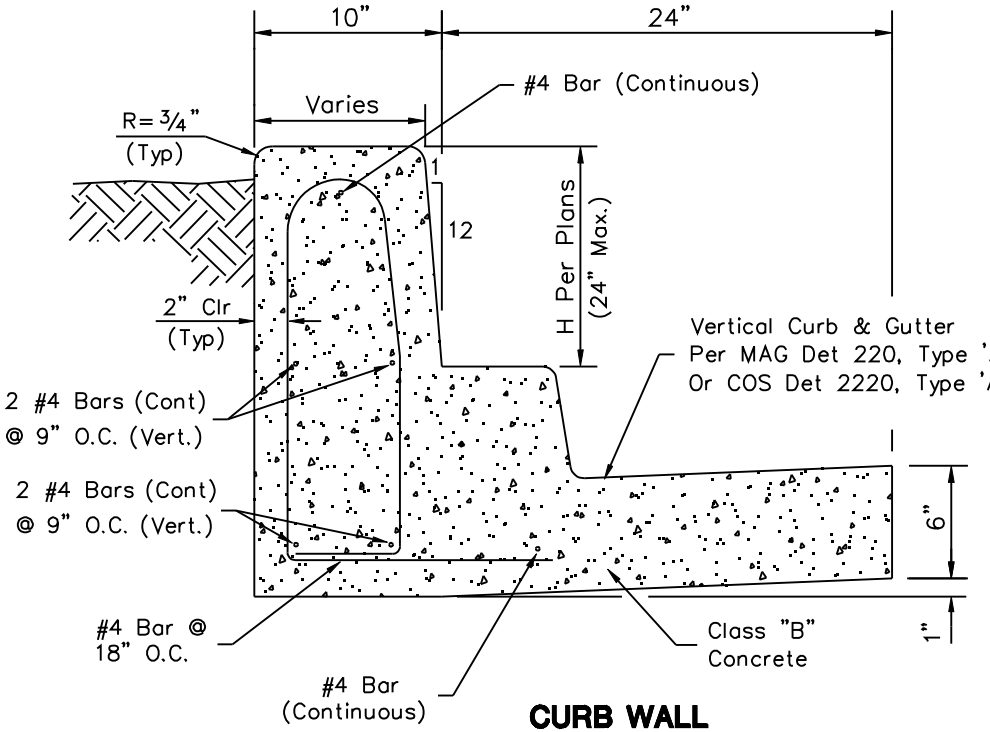


MOUNTABLE/MEDIAN CURB & GUTTER

TYPE "M"



MOUNTABLE CURB TO VERTICAL CURB TRANSITION



CURB WALL

TYPE "W"

NOTES

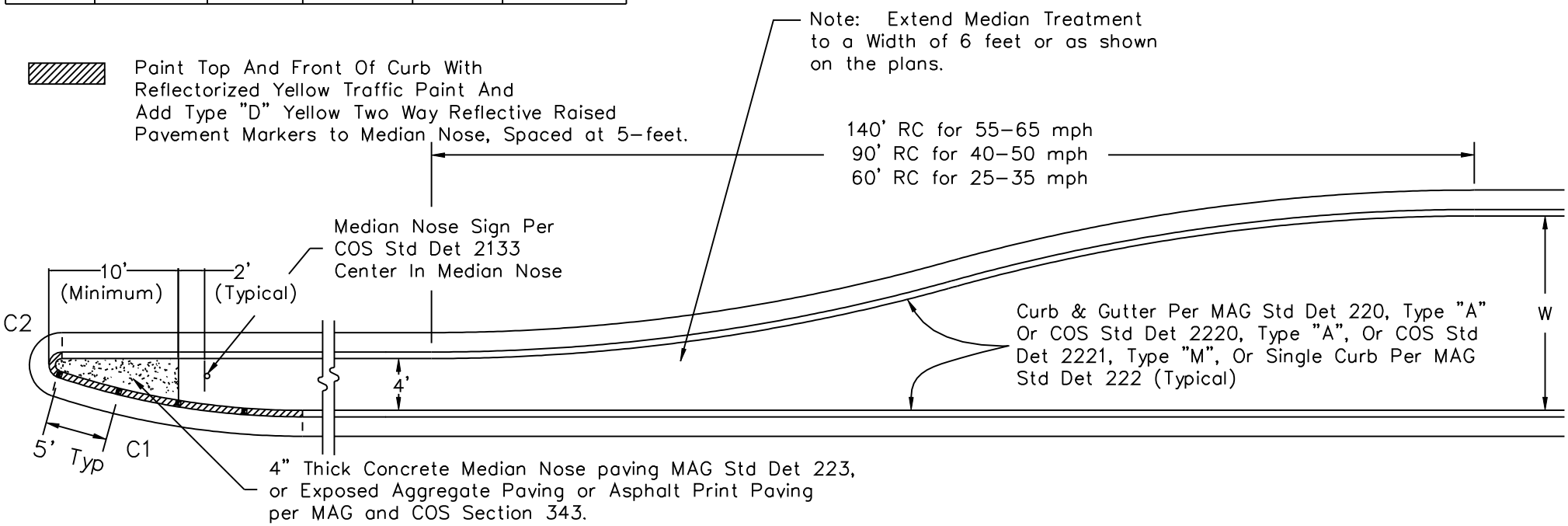
1. All exposed surfaces to be trowel finished except as shown. See M.A.G. Section 340.
2. Contraction joint spacing 10' maximum.
3. Construct curb and install ½" mastic expansion joints, A.S.T.M. D-1751, per M.A.G. Sec. 340 & 729 and C.O.S. Sec. 340.
4. Gutter lip may be depressed where indicated on plans and constructed as shown on COS Detail 2220, Type "A".
5. Colored concrete, if called for on the plans, shall be colored integrally.
6. Steel reinforcement Per M.A.G. Section 727.

REVISED 3/28/02

DETAIL NO. 2221	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	CURB & GUTTER - TYPES M & W	DETAIL NO. 2221

MEDIAN NOSE CURVE DATA

CURVE	RADIUS *	LENGTH	TANGENT	CHORD	DELTA
C1	59.5'	19.06'	9.61'	18.97'	18°20'59"
C2	0.50'	1.41'	3.10'	0.99'	161°39'01"

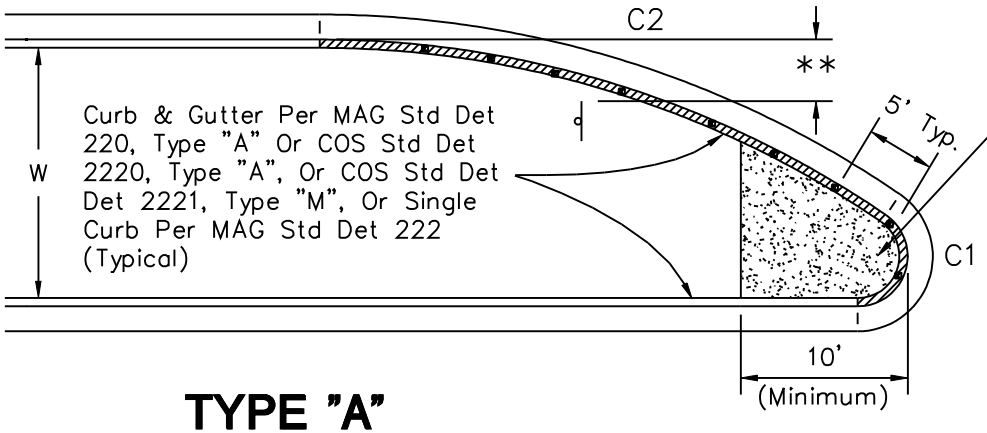


REVISED 5/17/06

* ALL RADII AND DIMENSIONS TO BACK OF CURB

NOTE: Curve Data Shown Is For Streets On Linear Alignments Only.

DETAIL NO. 2225	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	MEDIAN NOSE & REVERSE CURVE DETAILS	DETAIL NO. 2225



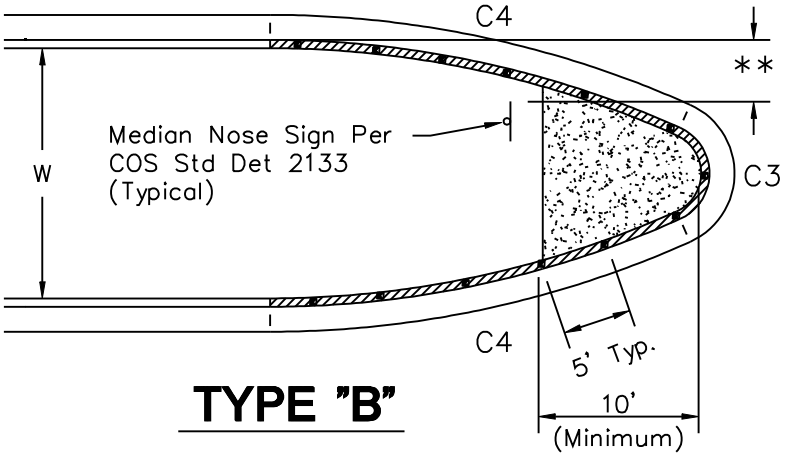
TYPE "A"

4" Thick Concrete Median Nose Paving MAG Std Det 223, or Exposed Aggregate Paving or Asphalt Print Paving per MAG and COS Section 343 (Typical)

* ALL RADII AND DIMENSIONS TO BACK OF CURB

** OFFSET TO BE NO MORE THAN 3' FROM FACE OF CURB AT TANGENT TO EDGE OF SIGN

Paint Top And Front Of Curb With Reflectorized Yellow Traffic Paint And Add Type "D" Yellow Reflective Raised Pavement Markers to Median Nose Nose, Spaced at 5-feet.



TYPE "B"

*CURVE DATA – W=15'					
CURVE	RADIUS	LENGTH	TANGENT	CHORD	DELTA
C1	2.50'	6.35'	8.06'	4.78'	145°32'39"
C2	59.50'	35.78'	18.45'	35.24'	34°27'21"
C3	2.50'	5.74'	5.57'	4.56'	131°38'42"
C4	59.50'	25.11'	12.74'	24.92'	24°10'39"

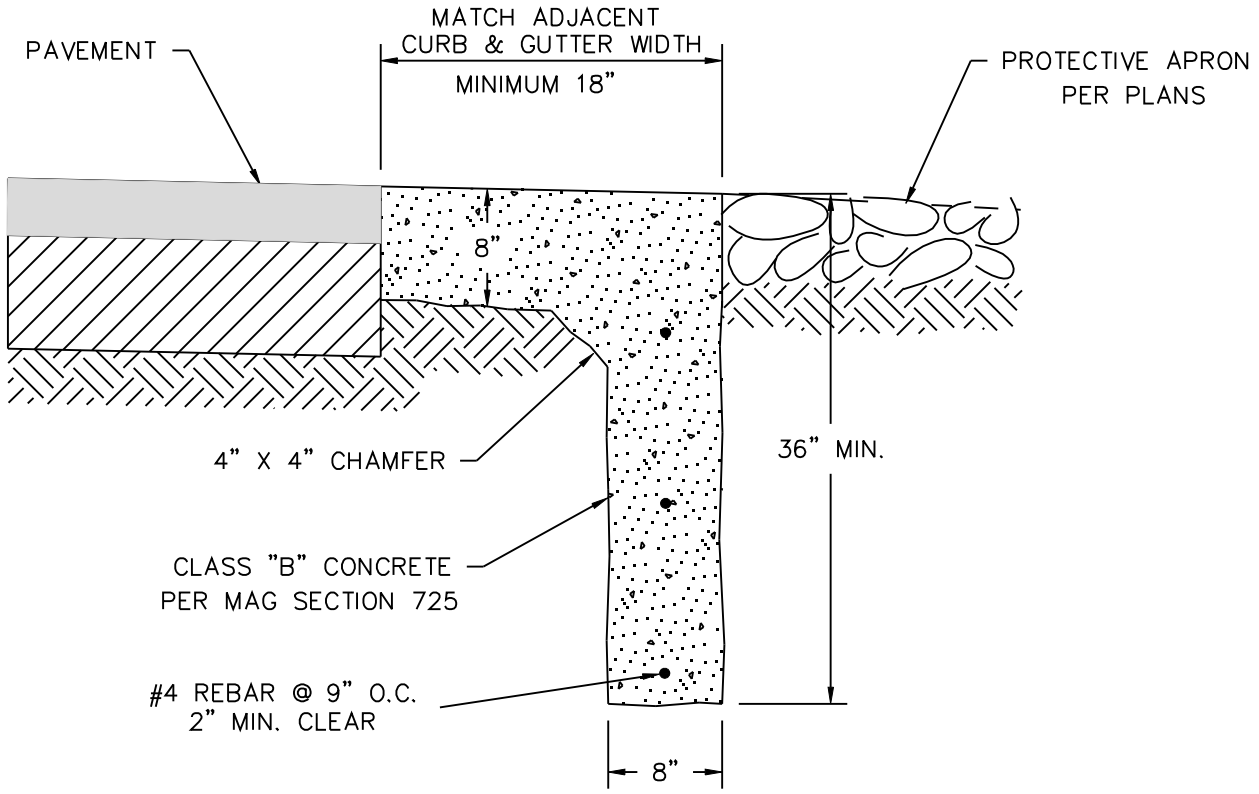
*CURVE DATA – W=16'					
CURVE	RADIUS	LENGTH	TANGENT	CHORD	DELTA
C1	2.50'	6.27'	7.65'	4.75'	143°48'20"
C2	59.50'	37.59'	19.44'	36.97'	36°11'40"
C3	2.50'	5.64'	5.27'	4.52'	129°14'46"
C4	59.50'	26.35'	13.40'	26.14'	25°22'37"

*CURVE DATA – W=24'					
CURVE	RADIUS	LENGTH	TANGENT	CHORD	DELTA
C1	2.50'	5.75'	5.59'	4.56'	131°48'37"
C2	59.50'	50.04'	26.61'	48.58'	48°11'23"
C3	2.50'	4.93'	3.77'	4.17'	112°53'08"
C4	59.50'	34.85'	17.94'	34.35'	33°33'26"

REVISED 3/28/02

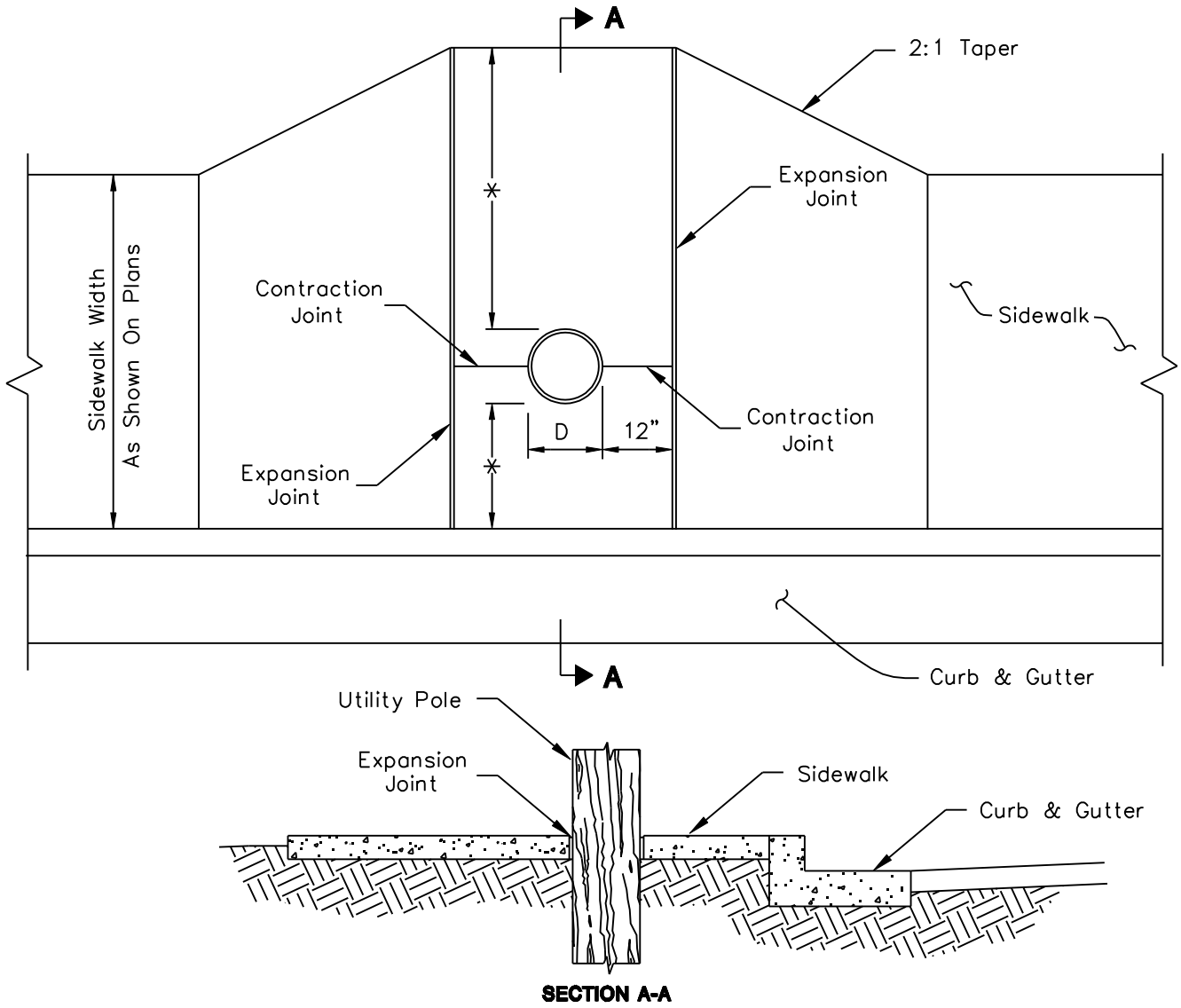
DETAIL NO. 2226	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	MEDIAN NOSE DETAILS	DETAIL NO. 2226

REVISED 2/22/99



NOTE: Form The Top 8" Of Cut-Off Wall

DETAIL NO. 2228	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	CUT-OFF WALL	DETAIL NO. 2228



SECTION A-A

NOTES

Install expansion joint around wood pole.

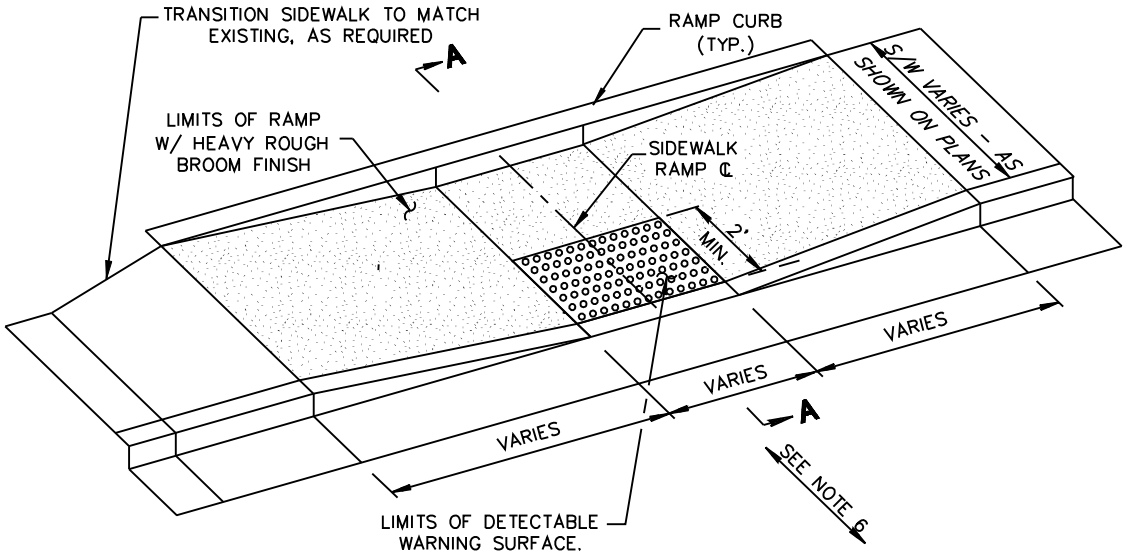
Install expansion joint at sidewalk 1/4" lower than sidewalk

* Maintain 48" clear on one side of utility pole unless otherwise approved by City staff.

D = Pole Diameter
Range: 12"–17"

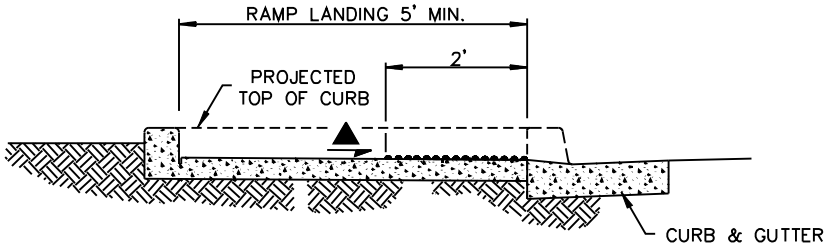
REVISED 4/18/05

DETAIL NO. 2230	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	SIDEWALK CUTOUT FOR UTILITY POLES	DETAIL NO. 2230

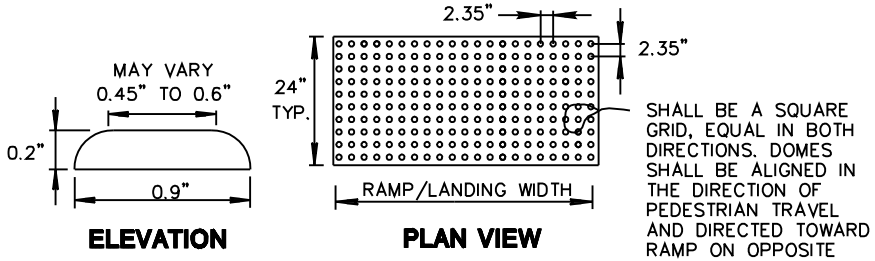


NOTES

1. DETECTABLE WARNING SHALL CONSIST OF RAISED TRUNCATED DOMES MANUFACTURED BY "COTE-L INDUSTRIES, INC.". CALLED "SAFTI-TRAX", WITH POLYURETHANE COATING "DURABAK", OR APPROVED EQUAL. APPLIED ON SMOOTH (NON-GROOVED) CLEAN CONCRETE RAMP, AND SHALL CONFORM TO THE DETAILS IN THE PLANS AND IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS AND INSTALLATION INSTRUCTIONS.
2. ALL DETECTABLE WARNING AREAS SHALL START AT BACK OF CURB, BE 24 INCHES IN DEPTH AND COVER THE COMPLETE WIDTH OF THE RAMP AREA 48 INCHES MIN.,
3. 70% VISUAL CONTRAST IS REQUIRED. THE COLOR SHALL BE AN INTEGRAL PART OF THE DETECTABLE WARNING MATERIAL, AS SPECIFIED ON THE PLANS. COLOR TO BE DETERMINED BY THE CITY STAFF, SAFETY YELLOW IS THE DEFAULT COLOR.
4. THE SMOOTH AND CLEAN CONCRETE UNDER DETECTABLE WARNING DEVICE AREA SHALL BE INCLUDED IN THE COST OF THE CONCRETE CURB RAMP. THE COST OF FURNISHING AND INSTALLING THE DETECTABLE WARNING DEVICE SHALL BE INCLUDED SEPARATELY AS "DETECTABLE WARNING DEVICE" PER SQUARE FOOT OR AS OUTLINED IN THE SPECIFICATIONS.
5. DETECTABLE WARNING SURFACE:
   APPLIED A COATING OF "DURABAK" SLIP-RESISTANT POLYURETHANE COATING TO THE SMOOTH, CLEAN CONCRETE SURFACE. ON TOP OF THE POLYURETHANE COATING APPLY TRUNCATED DOMES FROM A "SAFTY-TRAX" CONTACT SHEET. ON TOP OF THE TRUNCATED DOMES AND INITIAL POLYURETHANE COATING PLACE THREE ADDITIONAL COATS OF "DURABAK" POLYURETHANE COATING. COLOR TO BE DETERMINED BY CITY STAFF OR AS SPECIFIED ON THE PLANS. SAFETY YELLOW IS A DEFAULT COLOR.
6. ALL RAMPS AND DETECTABLE WARNING SHALL BE ALIGNED IN THE DIRECTION OF PEDESTRIAN TRAVEL AND DIRECTED TOWARD RAMP ON THE OPPOSITE SIDE OF STREET.



SECTION A-A

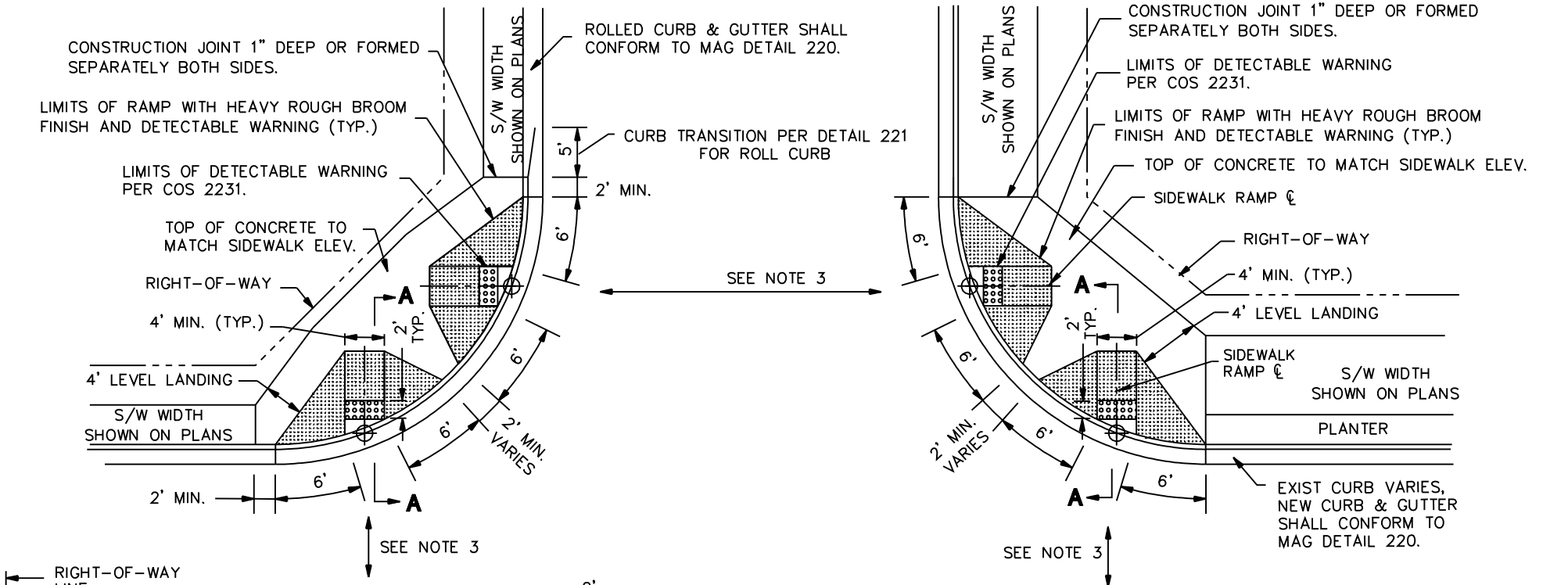


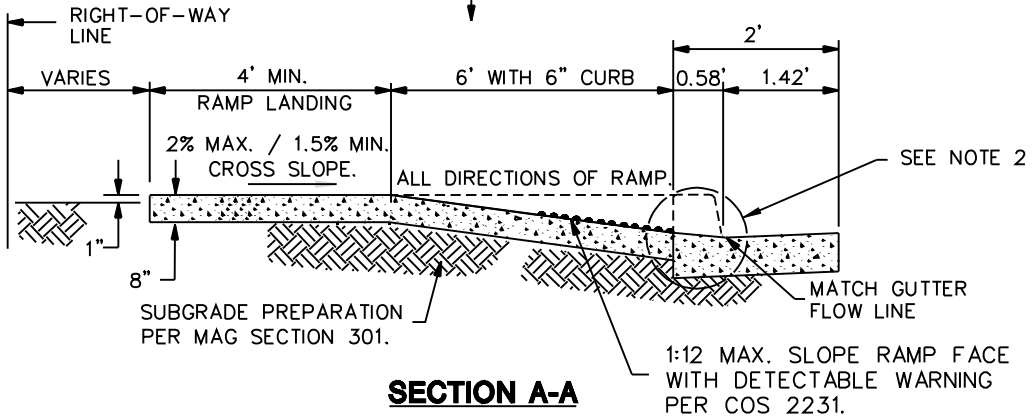
ELEVATION **PLAN VIEW**

TRUNCATED DOME DETECTABLE WARNING

REVISED 5/25/05

DETAIL NO. 2231	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	DETECTABLE WARNING SURFACE	DETAIL NO. 2231





SECTION A-A

LEGEND

⊕ RAMP CONTROL POINT (TYP.) SEE PLANS

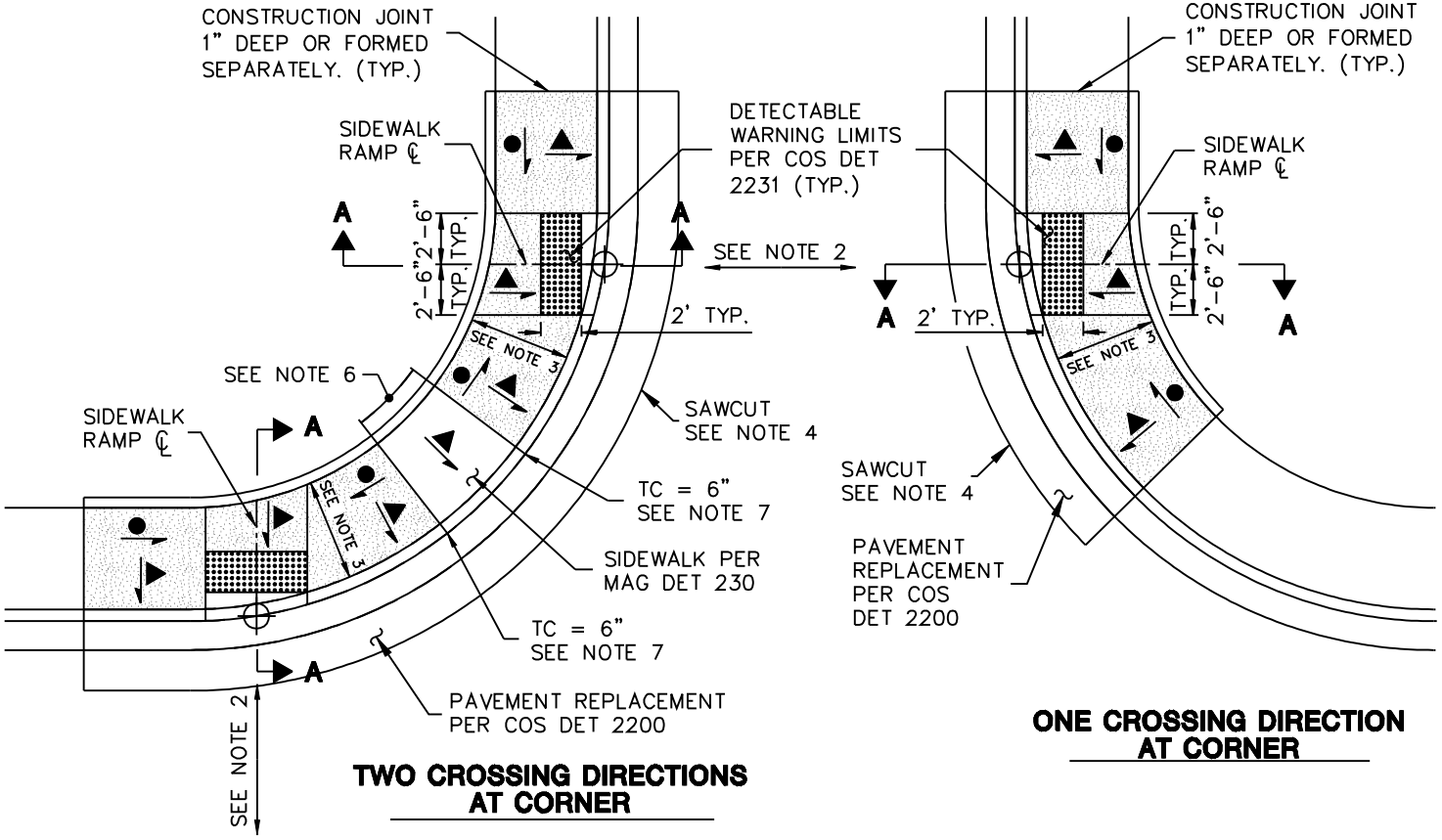
NOTES:

1. ALL CONCRETE TO BE CLASS "B", MAG SECTION 725.
2. FOR SLOPING TRANSITION FROM RAMP TO CURB, SEE DETAIL 1 COS 2235.
3. ALL RAMPS AND DETECTABLE WARNING SHALL BE ALIGNED IN THE DIRECTION OF PEDESTRIAN TRAVEL AND DIRECTED TOWARD RAMP ON OPPOSITE SIDE OF STREET.
4. THIS IS THE PREFERED ALTERNATIVE FOR SIDEWALK RAMPS. THE USE OF ANY SIDEWALK RAMP OTHER THAN DIRECTIONAL SIDEWALK RAMPS REQUIRES APPROVAL FROM CITY STAFF.
5. SEE PLANS FOR LOCATION OF SIDEWALK RAMP CENTERLINE

REVISED 5/25/05

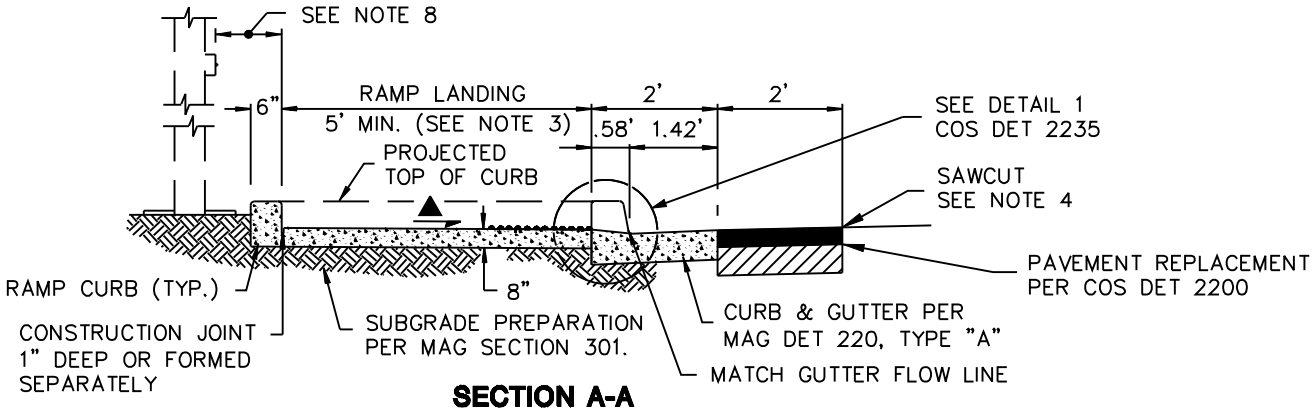
DETAIL NO. 2232	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	DIRECTIONAL SIDEWALK RAMPS	DETAIL NO. 2232

REVISED 5/25/05



NOTES:

1. ALL CONCRETE TO BE CLASS "B", MAG SECTION 725.
2. ALL RAMPS AND DETECTABLE WARNING SHALL BE ALIGNED IN THE DIRECTION OF PEDESTRIAN TRAVEL AND DIRECTED TOWARD RAMP ON THE OPPOSITE SIDE OF STREET.
3. 5' MINIMUM MEASURED RADIALLY FROM BACK OF CURB TO THE FACE OF RAMP CURB.
4. WHEN A CONCRETE APRON EXISTS THE SAWCUT MAY BE MADE IN THE APRON 2' FROM BACK OF EXISTING CURB.
5. SEE PLANS FOR LOCATION OF SIDE-WALK RAMP CENTERLINE.
6. MINIMUM 4' LONG LEVEL AREA REQUIRED BETWEEN RAMPS.
7. CURB HEIGHT MAY BE DECREASED TO 4" FOR SPACE LIMITED AREAS PROVIDED THE RAMP SLOPE IS A MAXIMUM 12:1 AND THE MINIMUM 4' LONG LEVEL AREA BETWEEN RAMPS IS MAINTAINED.
8. PROVIDE 10" MAXIMUM TO FACE OF ANY EXISTING PEDESTRIAN PUSH BUTTON. RAMP CURB AND LANDING MAY NEED TO BE EXTENDED AROUND POLE TO PROVIDE ACCESS TO PUSH BUTTON.
9. EACH RAMP RETROFIT REQUIRES A SITE SPECIFIC ASSESSMENT AND FIELD RE-VIEW BY THE DESIGN ENGINEER TO ENSURE FUNCTIONAL DESIGN.

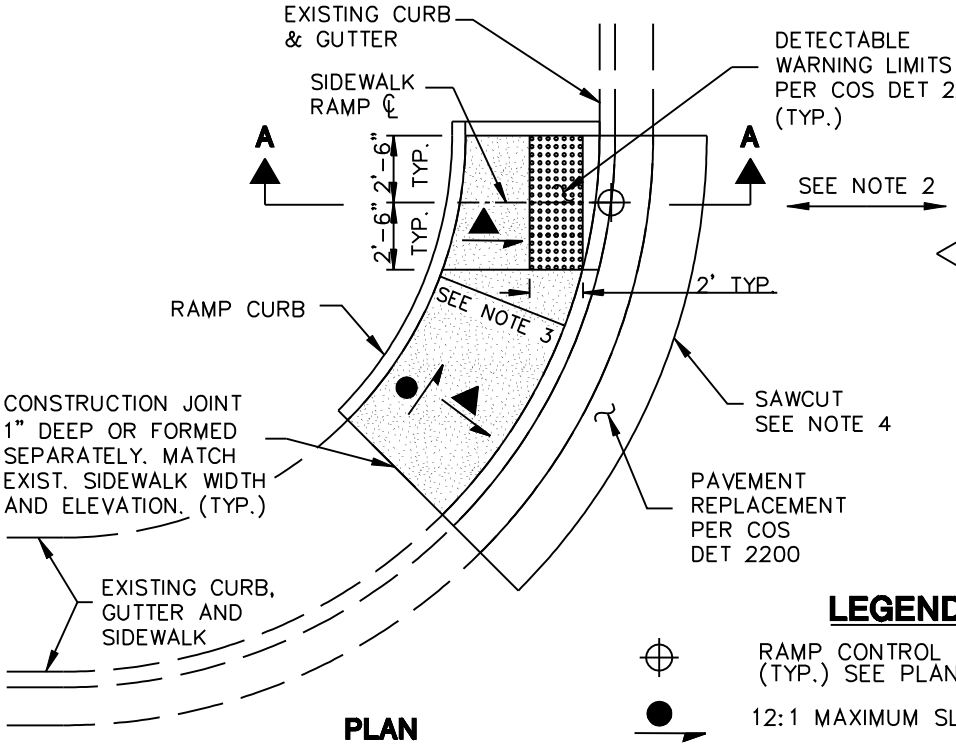


SECTION A-A

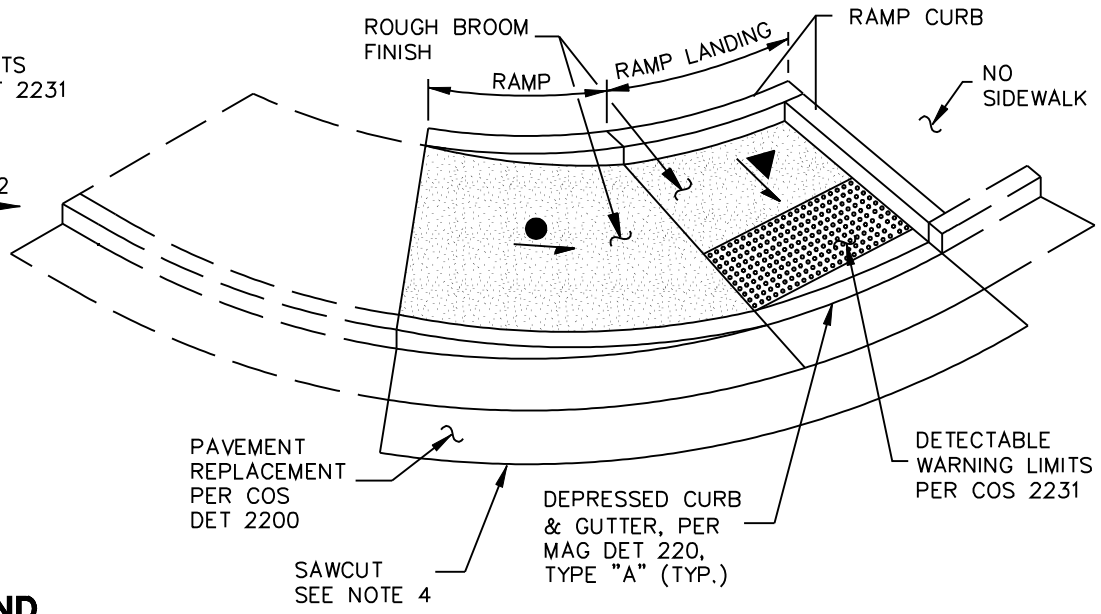
LEGEND

- RAMP CONTROL POINT (TYP.) SEE PLANS
- 12:1 MAXIMUM SLOPE, 15:1 DESIRED SLOPE
- 2% MAXIMUM SLOPE, 1.5% MINIMUM SLOPE
- SIDEWALK RAMP PAYMENT LIMITS AND HEAVY ROUGH BROOM FINISH LIMITS

DETAIL NO.		APPROVED BY:		DETAIL NO.
2233-1	City of Scottsdale Standard Details	Scottsdale Standards & Specifications Committee	DIRECTIONAL SIDEWALK RAMP RETROFIT - TYPE A	2233-1



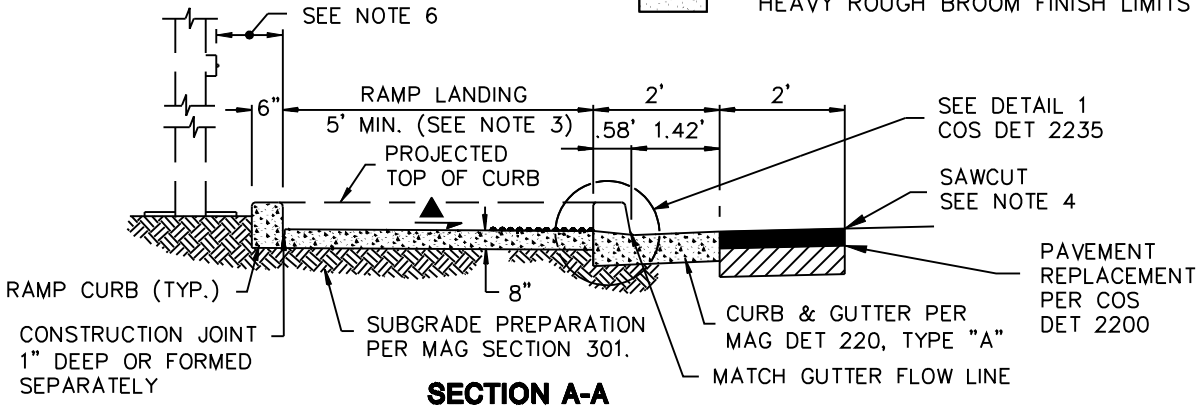
PLAN



PERSPECTIVE

LEGEND

- RAMP CONTROL POINT (TYP.) SEE PLANS
- 12:1 MAXIMUM SLOPE, 15:1 DESIRED SLOPE
- 2% MAXIMUM SLOPE, 1.5% MINIMUM SLOPE
- SIDEWALK RAMP PAYMENT LIMITS AND HEAVY ROUGH BROOM FINISH LIMITS



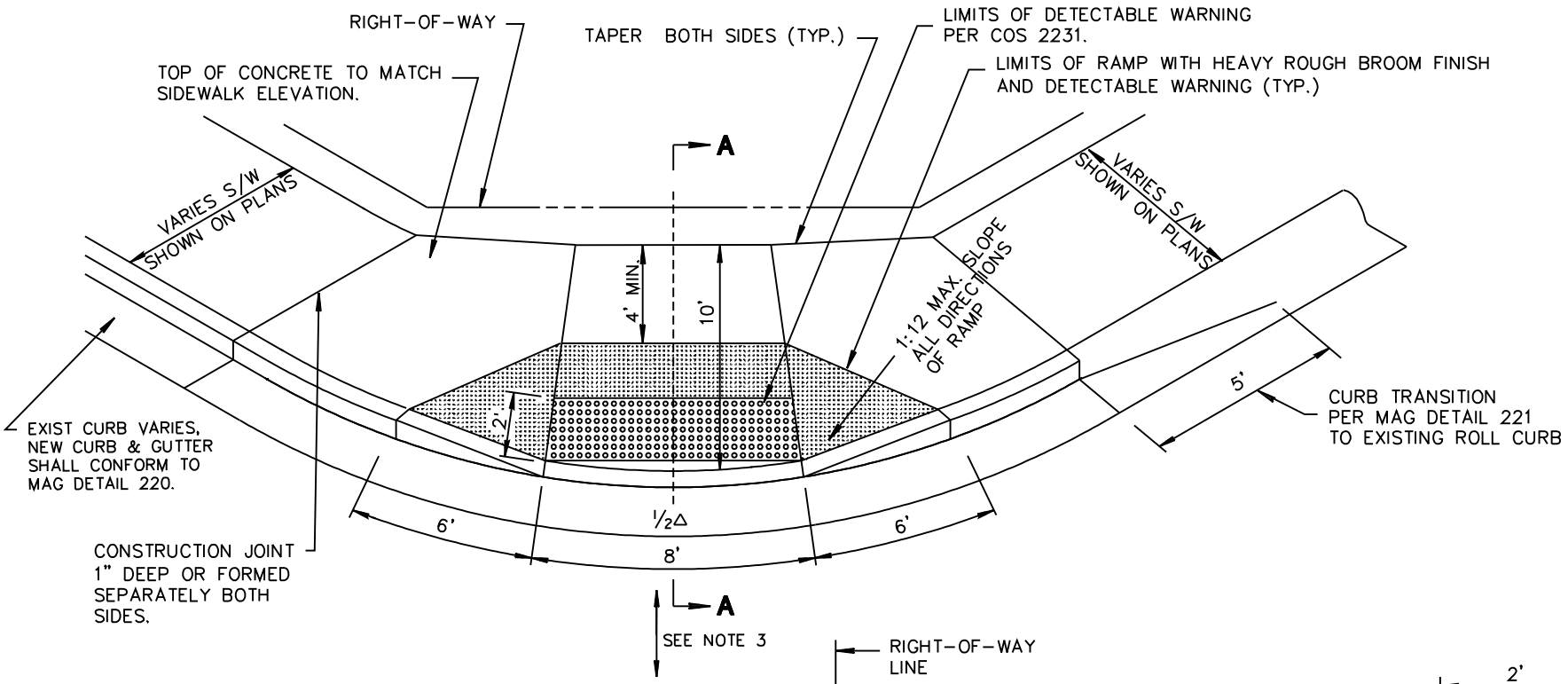
SECTION A-A

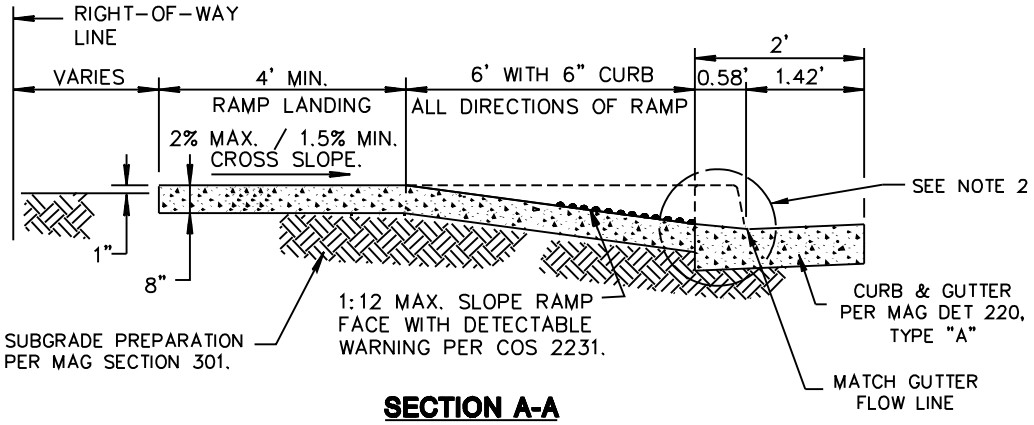
NOTES:

1. ALL CONCRETE TO BE CLASS "B", MAG SECTION 725.
2. ALL RAMPS AND DETECTABLE WARNING SHALL BE ALIGNED IN THE DIRECTION OF PEDESTRIAN TRAVEL AND DIRECTED TOWARD RAMP ON THE OPPOSITE SIDE OF STREET.
3. 5' MINIMUM MEASURED RADIALLY FROM BACK OF CURB TO FACE OF RAMP CURB.
4. WHEN A CONCRETE APRON EXISTS THE SAWCUT MAY BE MADE IN THE APRON 2' FROM BACK OF EXISTING CURB.
5. SEE PLANS FOR LOCATION OF SIDEWALK RAMP CENTER LINE.
6. PROVIDE 10" MAXIMUM TO FACE OF ANY EXISTING PEDESTRIAN PUSH BUTTON. RAMP CURB AND LANDING MAY NEED TO BE EXTENDED AROUND POLE TO PROVIDE ACCESS TO PUSH BUTTON.
7. THIS SIDEWALK RAMP DETAIL IS FOR RETROFITTING ONLY AND IS NOT TO BE USED FOR NEW CONSTRUCTION.
9. EACH RAMP RETROFIT REQUIRES A SITE SPECIFIC ASSESSMENT AND FIELD REVIEW BY THE DESIGN ENGINEER TO ENSURE FUNCTIONAL DESIGN.

REVISED 5/25/05

DETAIL NO.		APPROVED BY:		DETAIL NO.
2233-2	City of Scottsdale Standard Details	Scottsdale Standards & Specifications Committee	DIRECTIONAL SIDEWALK RAMP RETROFIT - TYPE B	2233-2





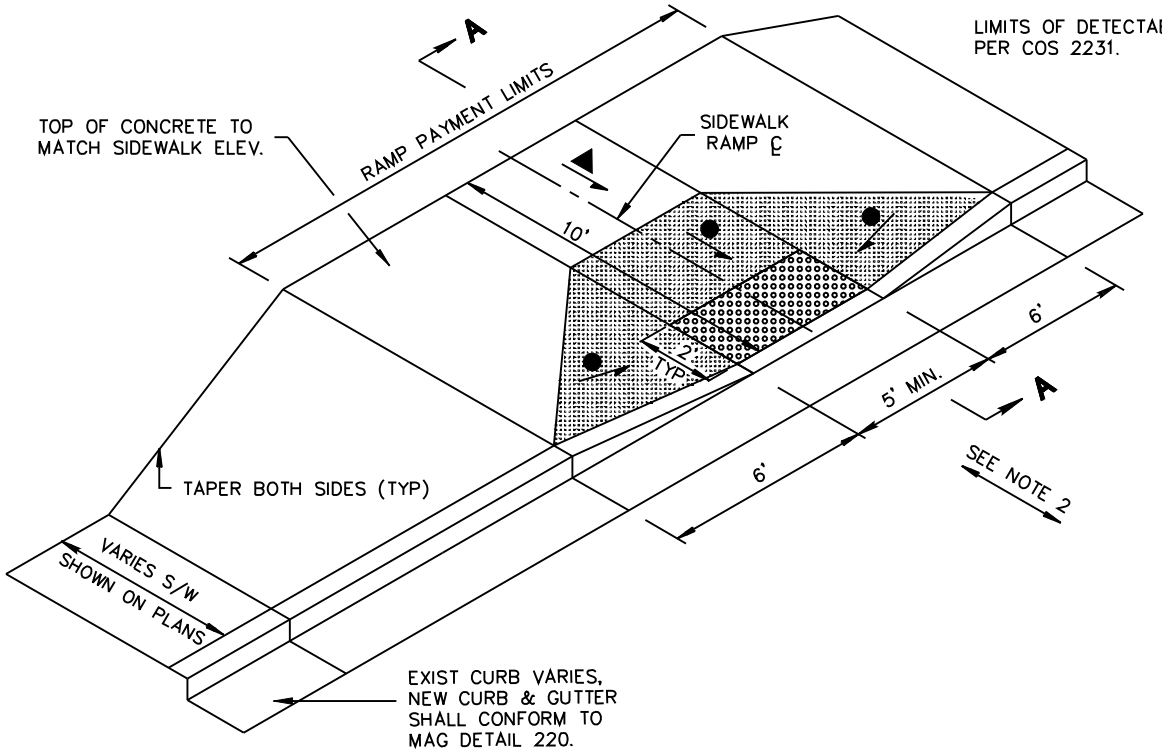
SECTION A-A

NOTES:

1. ALL CONCRETE TO BE CLASS "B", MAG SECTION 725.
2. FOR SLOPING TRANSITION FROM RAMP TO CURB, SEE DETAIL 1 COS 2235.
3. ALL RAMPS AND DETECTABLE WARNING SHALL BE ALIGNED IN THE DIRECTION OF PEDESTRIAN TRAVEL AND DIRECTED TOWARD RAMP ON OPPOSITE SIDE OF STREET.
4. USE OF THIS RAMP IS NOT PREFERRED. IT SHALL BE USED ONLY WHERE DIRECTIONAL RAMPS ARE NOT POSSIBLE.

REVISED 5/25/05

DETAIL NO. 2234	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	SHARED CURB SIDEWALK RAMP	DETAIL NO. 2234



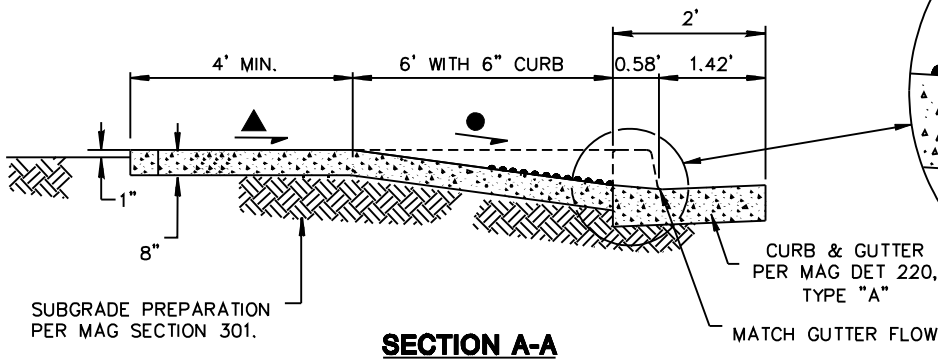
LIMITS OF DETECTABLE WARNING PER COS 2231.

NOTES:

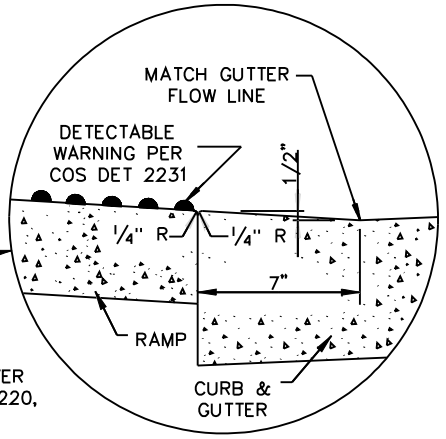
1. ALL CONCRETE TO BE CLASS "B", MAG SECTION 725.
2. ALL RAMPS AND DETECTABLE WARNING SHALL BE ALIGNED IN THE DIRECTION OF PEDESTRIAN TRAVEL AND DIRECTED TOWARD RAMP ON THE OPPOSITE SIDE OF STREET.
3. SEE PLANS FOR LOCATION OF SIDEWALK RAMP CENTERLINE.

LEGEND

- 12:1 MAXIMUM SLOPE, 15:1 DESIRED SLOPE
- 2% MAXIMUM SLOPE, 1.5% MINIMUM SLOPE
- SIDEWALK RAMP PAYMENT LIMITS AND HEAVY ROUGH BROOM FINISH LIMITS



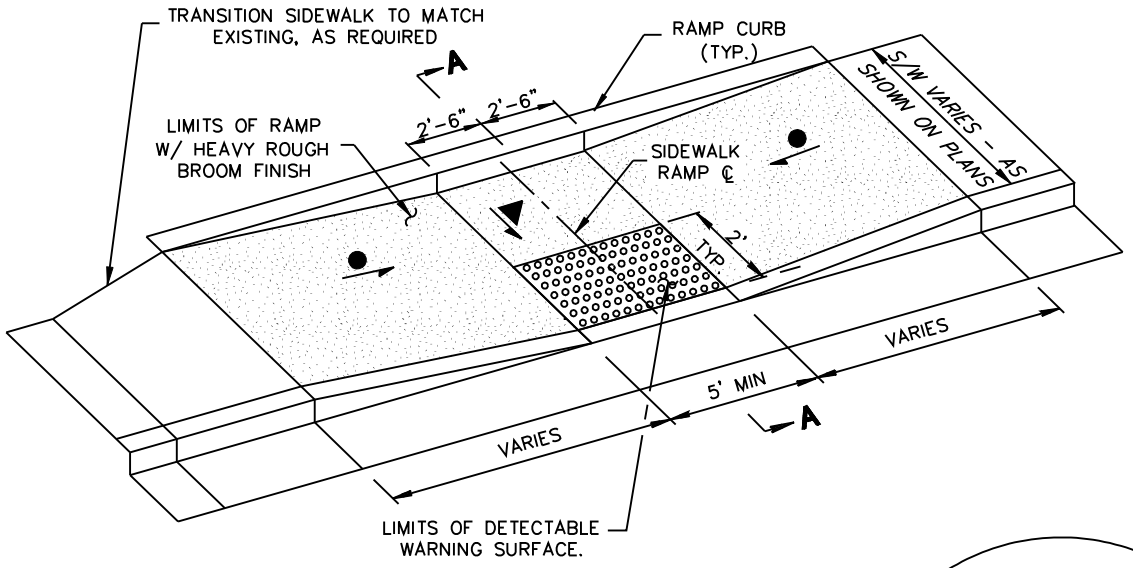
SECTION A-A



DETAIL 1

REVISED 5/25/05

DETAIL NO.		APPROVED BY:		DETAIL NO.
2235-1	City of Scottsdale Standard Details	Scottsdale Standards & Specifications Committee	MID-BLOCK SIDEWALK RAMP - TYPE A	2235-1

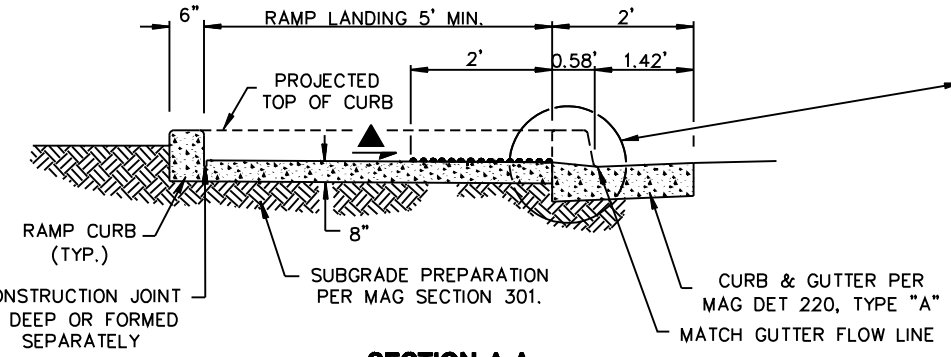


NOTES:

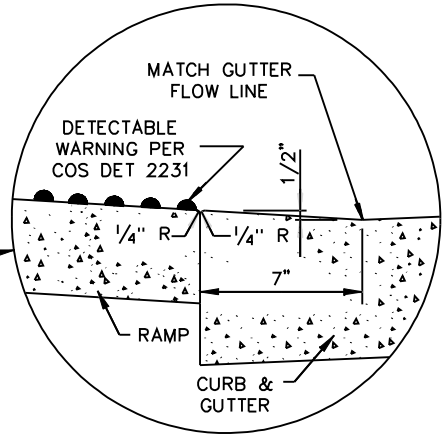
1. ALL CONCRETE TO BE CLASS "B", MAG SECTION 725.
2. ALL RAMPS AND DETECTABLE WARNING SHALL BE ALIGNED IN THE DIRECTION OF PEDESTRIAN TRAVEL AND DIRECTED TOWARD RAMP ON THE OPPOSITE SIDE OF STREET.
3. SEE PLANS FOR LOCATION OF SIDEWALK RAMP CENTERLINE.

LEGEND

- 12:1 MAXIMUM SLOPE, 15:1 DESIRED SLOPE
- 2% MAXIMUM SLOPE, 1.5% MINIMUM SLOPE
- SIDEWALK RAMP PAYMENT LIMITS AND HEAVY ROUGH BROOM FINISH LIMITS



SECTION A-A



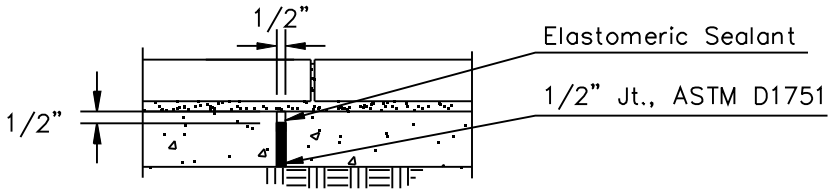
DETAIL 1

REVISED 5/25/05

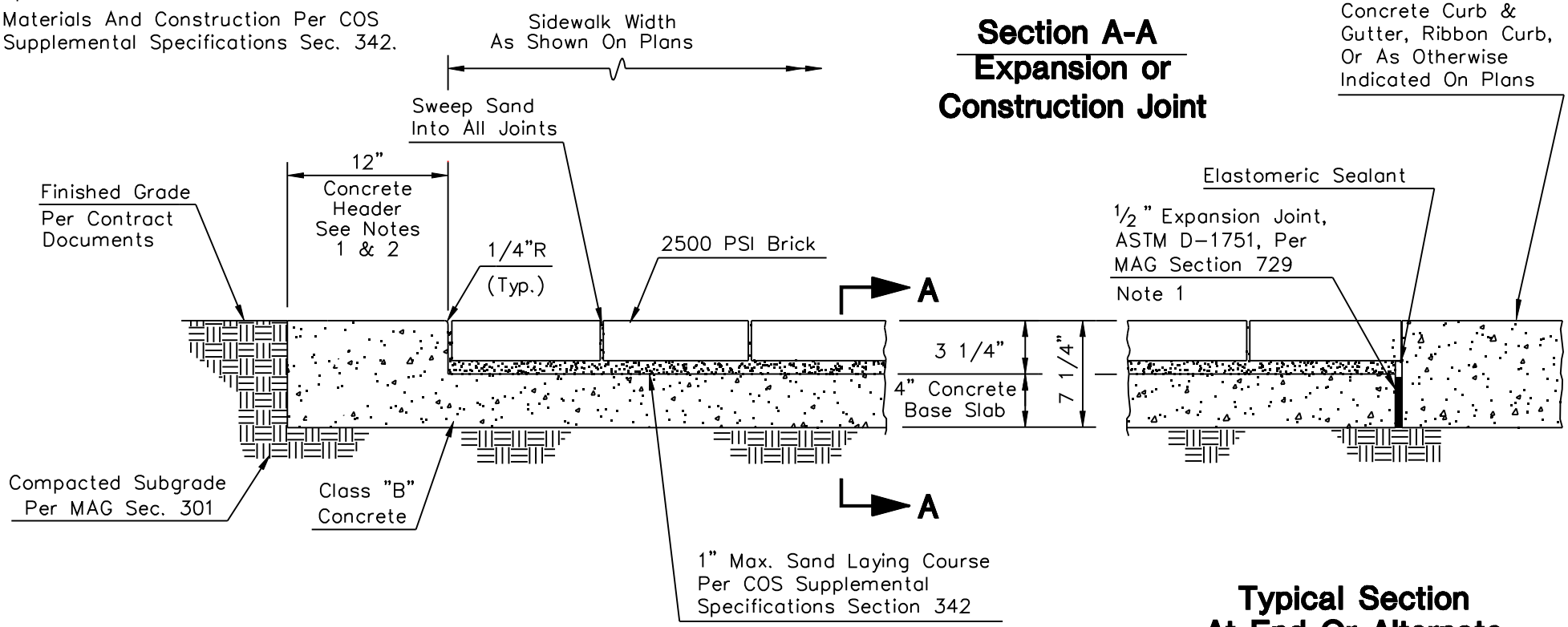
DETAIL NO.		APPROVED BY:		DETAIL NO.
2235-2	City of Scottsdale Standard Details	Scottsdale Standards & Specifications Committee	MID-BLOCK SIDEWALK RAMP - TYPE B	2235-2

NOTES:

1. ½" Expansion Joint, ASTM D-1751 Per MAG Sec. 729, and Elastomeric Sealant Per COS Sec. 342.3.4.
2. Contraction Joints Per COS Supplemental Specifications Sec. 342.
3. Materials And Construction Per COS Supplemental Specifications Sec. 342.



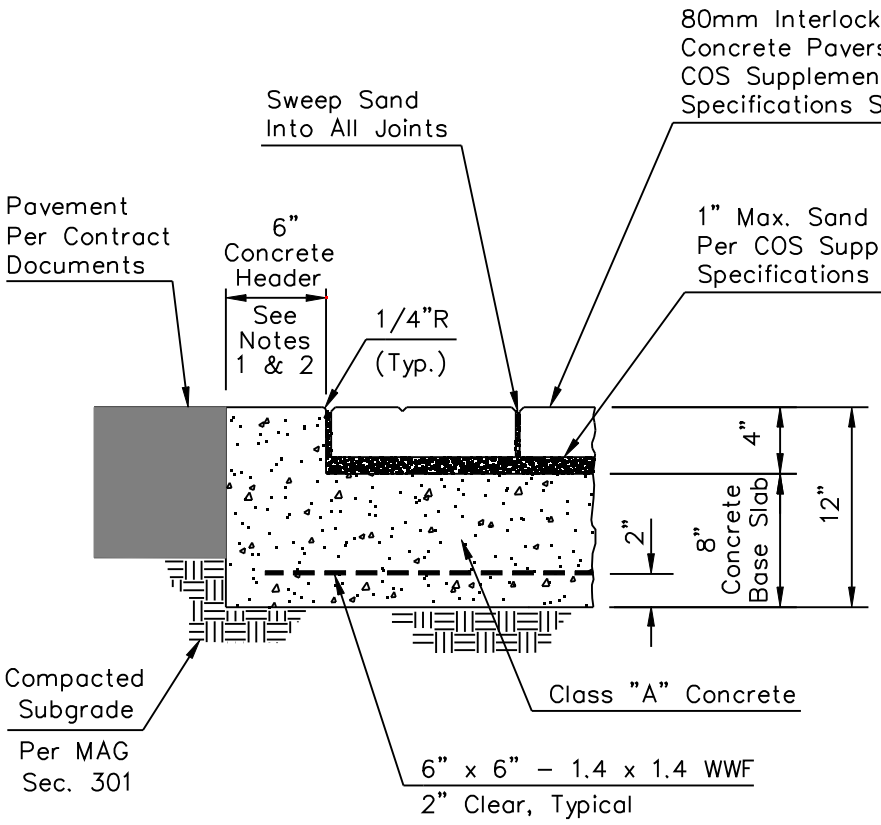
Section A-A Expansion or Construction Joint

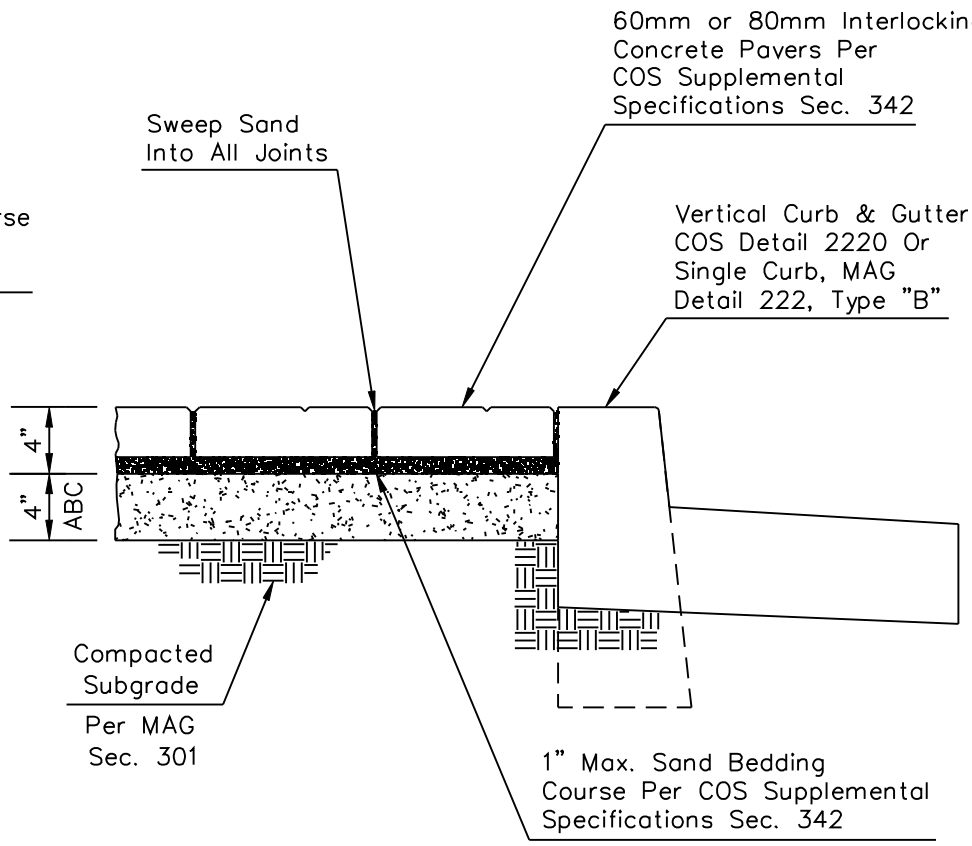


Typical Half Section (Against Earth)

Typical Section At End Or Alternate Half Section (Against Concrete)

DETAIL NO.		APPROVED BY:		DETAIL NO.
2237	City of Scottsdale Standard Details	Scottsdale Standards & Specifications Committee	SIDEWALK PAVERS (NON-TRAFFIC BEARING)	2237





REVISED 2/26/01

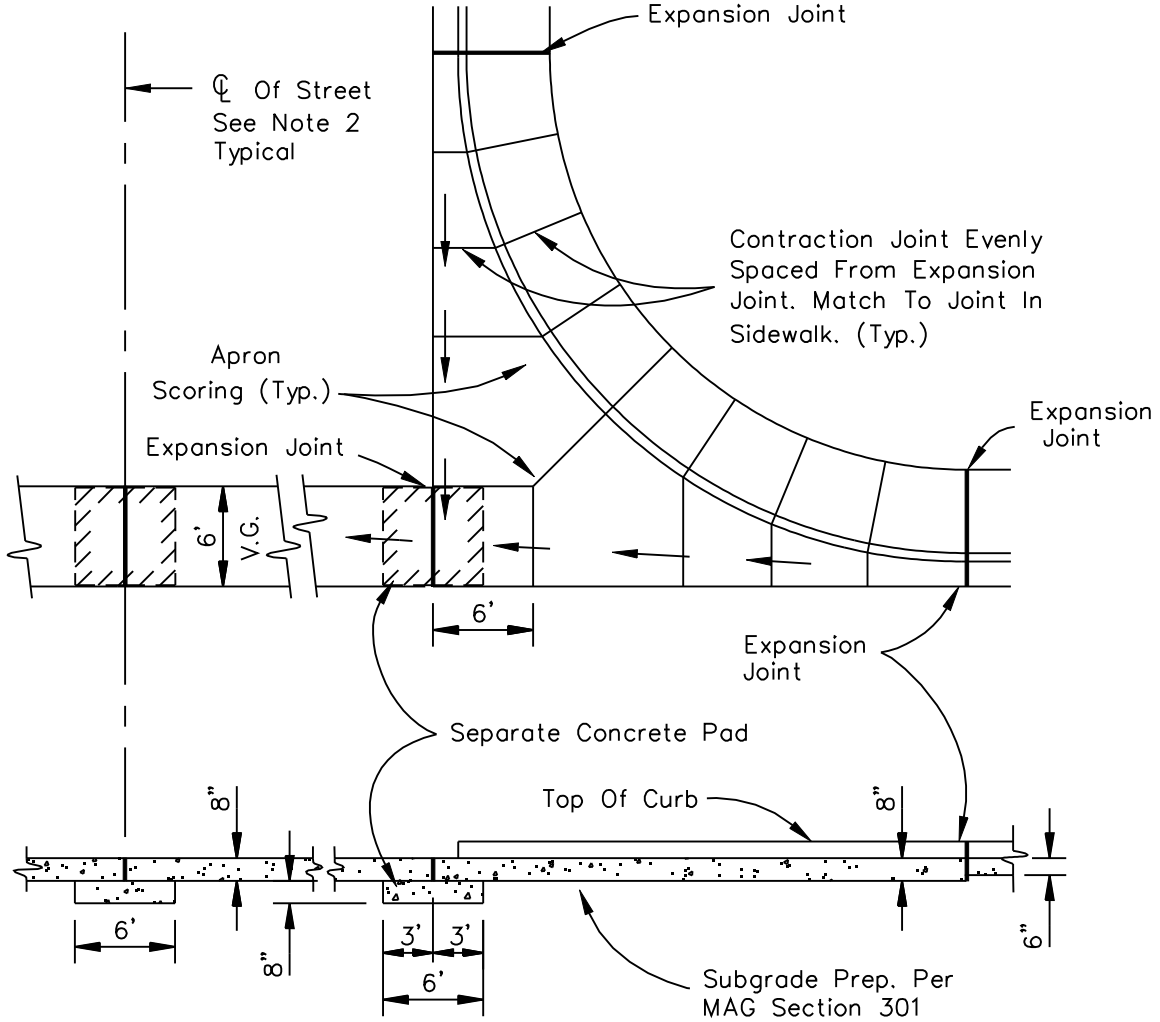
Type "B" Flush Median

Type "A" Raised Median

NOTES:

1. ½" Expansion Joint, ASTM D–1751 Per MAG Sec. 729, Every 50'.
2. Contraction Joints Per COS Supplemental Specifications Sec. 342, Every 10'.
3. Materials And Construction Per COS Supplemental Specifications Sec. 342.

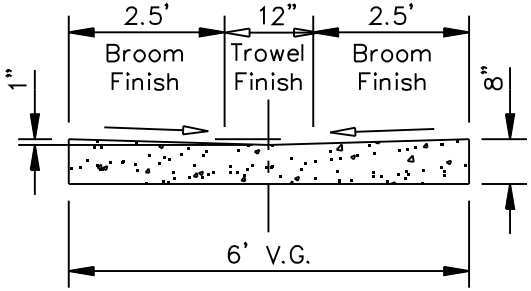
DETAIL NO. 2239	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	MEDIAN CONCRETE PAVERS	DETAIL NO. 2239



SECTION THRU APRON FLOW-LINE

NOTES

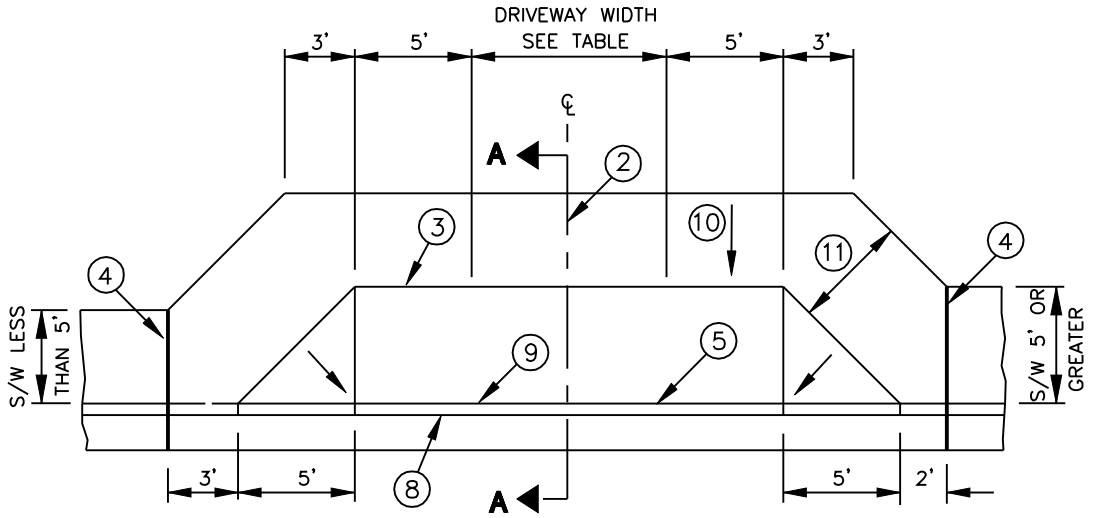
1. All Concrete To Be Class "A", MAG Section 725.
2. Use A Construction Joint Or Contraction Joint At The ℄ Of Street. A Separate Concrete Pad Is Required With A Construction Joint.
3. ½" Expansion Joint, ASTM D-1751 Per MAG Section 729.
4. Return Curb and Sidewalk to be Monolithically Poured.



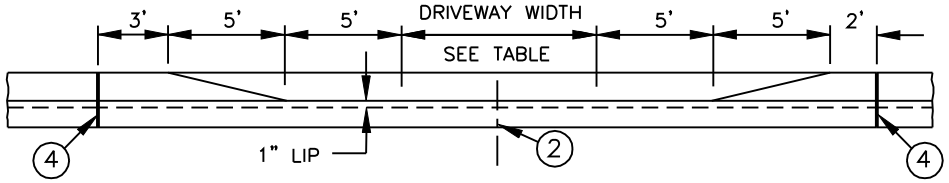
SECTION THRU VALLEY GUTTER

REVISED 5/10/05

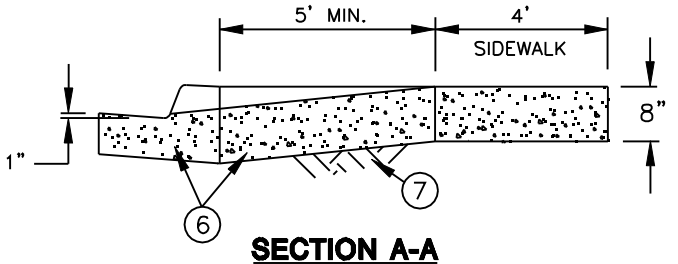
DETAIL NO. 2240	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	6' VALLEY GUTTER & APRON	DETAIL NO. 2240



PLAN



ELEVATION



SECTION A-A

REVISED 5/10/05

NOTES

1 DEPRESSED CURB SHALL BE PAID FOR AT THE UNIT PRICE BID FOR THE TYPE OF CURB USED AT THAT LOCATION.

2 CONTRACTION JOINT ON DRIVEWAY CENTERLINE.

3 BACK OF DRIVEWAY ENTRANCE – CONSTRUCTION JOINT OR SCORE MARK.

4 MASTIC EXPANSION JOINT THROUGH CURB AND GUTTER. EXPANSION JOINT FILLER SHALL BE 1/2" BITUMINOUS TYPE PREFORMED EXPANSION JOINT FILLER A.S.T.M. D–1751.

5 BACK OF CURB – CONSTRUCTION JOINT OR SCORE MARK.

6 CLASS 'B' CONCRETE, MAG SECTION 725.

7 SUBGRADE PREPARATION, MAG SECTION 301.

8 FLOW LINE OF GUTTER.

9 DEPRESSED CURB.

10 2% MAXIMUM CROSS SLOPE. 1.5% MINIMUM CROSS SLOPE

11 CONCRETE SIDEWALK PER MAG DETAIL 230, MODIFIED. THICKNESS = 8"

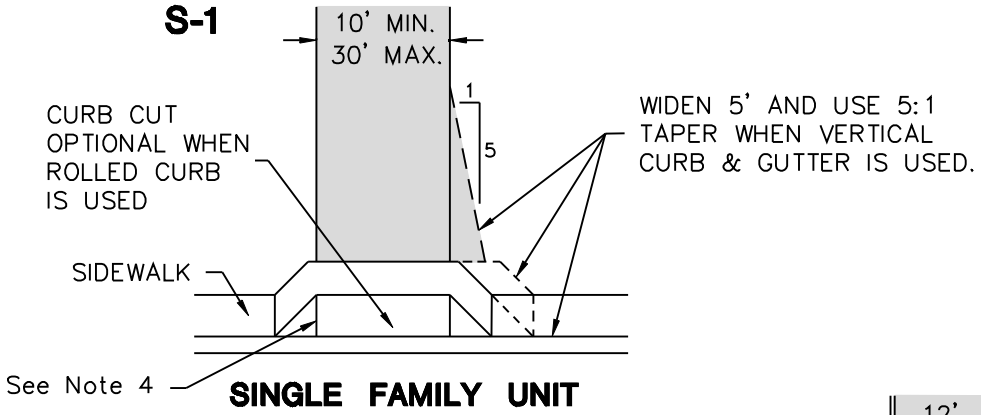
COMMERCIAL & INDUSTRIAL			
DRIVEWAY WIDTH	MIN.	MAX.	CLASS
COMMERCIAL ZONING	+ 16'	40'	B
INDUSTRIAL ZONING	+ 16'	40'	B
+ 24' MIN. FOR TWO WAY TRAFFIC			
RESIDENTIAL			
DRIVEWAY WIDTH	MIN.	MAX.	CLASS
MAJOR STREET	16'	30'	B
COLLECTOR STREET	*12'	30'	B
LOCAL STREET	12'	30'	B
* 16' DESIRABLE			

DETAIL NO. 2250 | City of Scottsdale Standard Details | APPROVED BY: Scottsdale Standards & Specifications Committee

DRIVEWAY ENTRANCES

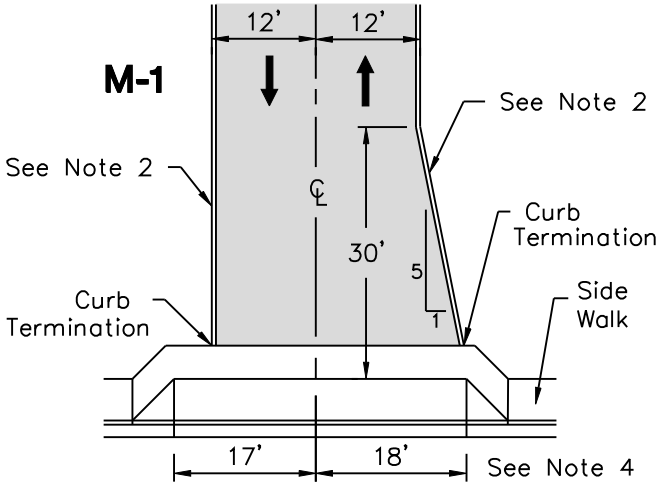
DETAIL NO. 2250

S-1



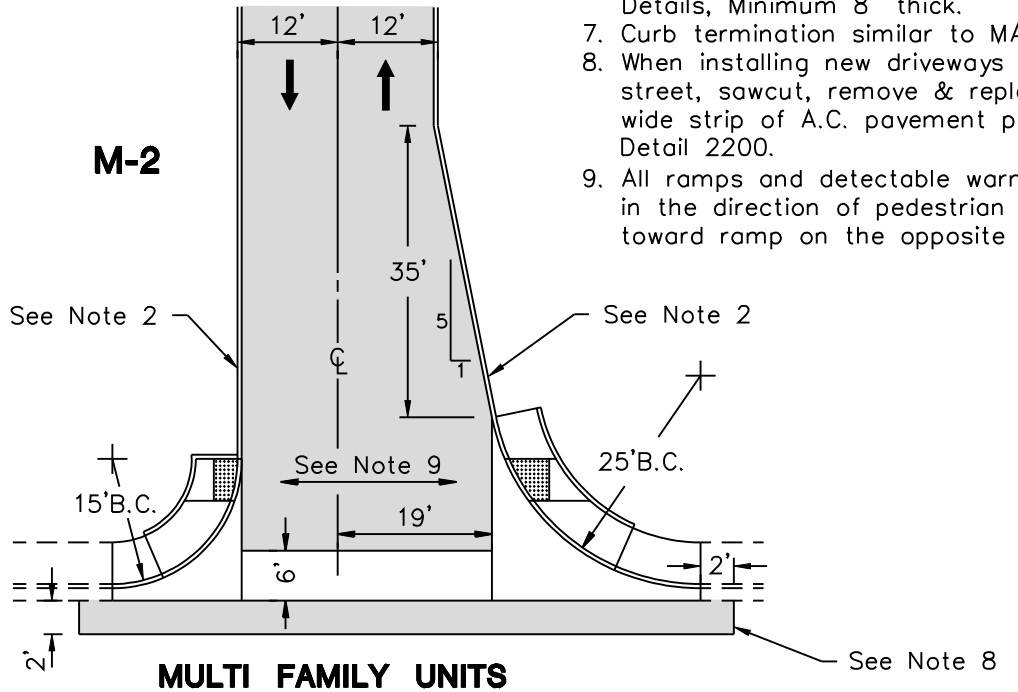
SINGLE FAMILY UNIT

M-1



MULTI FAMILY UNITS

M-2



MULTI FAMILY UNITS

CONSTRUCTION NOTES

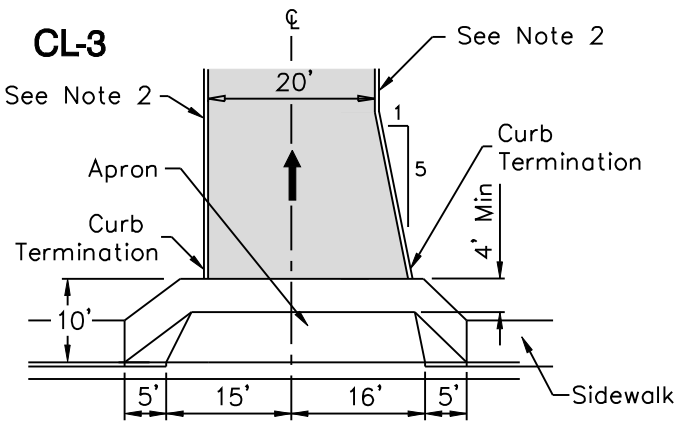
1. Sidewalk shall conform to MAG Standard Detail 230 and C.O.S. Sec. 340.
2. Curb as called for on plans.
3. Curb & Gutter shall conform to MAG Standard Detail 220. Single Curb shall conform to MAG Standard Detail 222.
4. Driveway Entrances shall conform to COS Standard Detail 2250. Driveways shall be 2–1/2" A.C. R–19 on 6" A.B.C. unless otherwise noted. See plans for driveway limits.
5. Valley Gutter shall conform to COS Standard Detail 2240.
6. Sidewalk Ramps shall conform to COS Standard Details, Minimum 8" thick.
7. Curb termination similar to MAG Std. Det 222.
8. When installing new driveways along an existing street, sawcut, remove & replace a minimum 2' wide strip of A.C. pavement per COS Standard Detail 2200.
9. All ramps and detectable warning shall be aligned in the direction of pedestrian travel and directed toward ramp on the opposite side of the street.

REVISED 5/10/05

DETAIL NO. 2255	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	RESIDENTIAL DRIVEWAYS	DETAIL NO. 2255

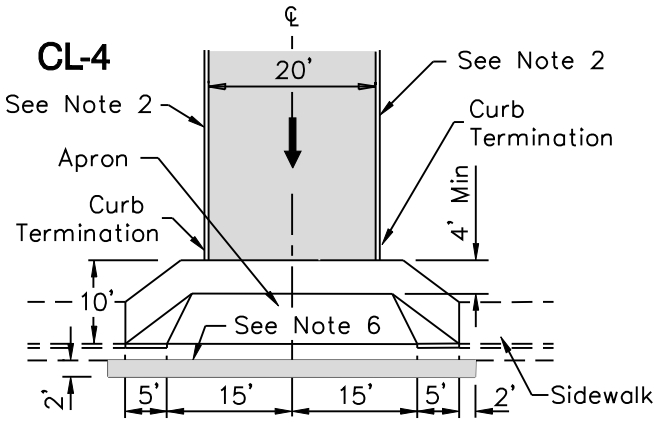
REVISED 5/17/06

CL-3



ONE WAY INGRESS

CL-4

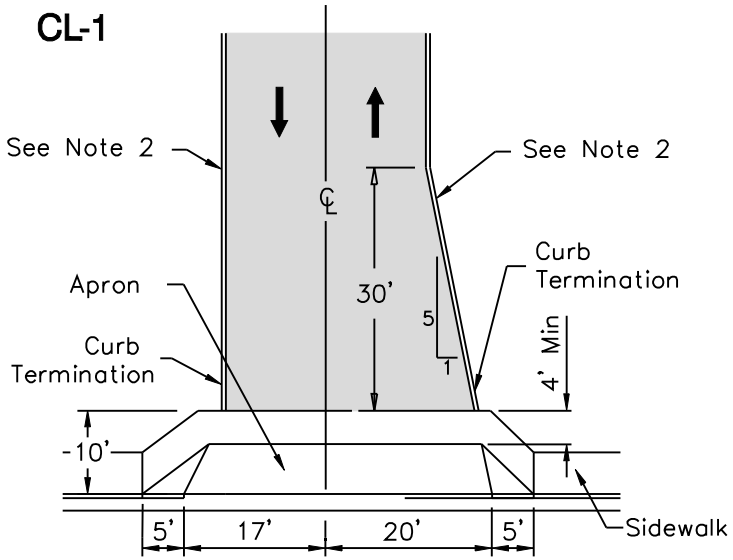


ONE WAY EGRESS

CONSTRUCTION NOTES

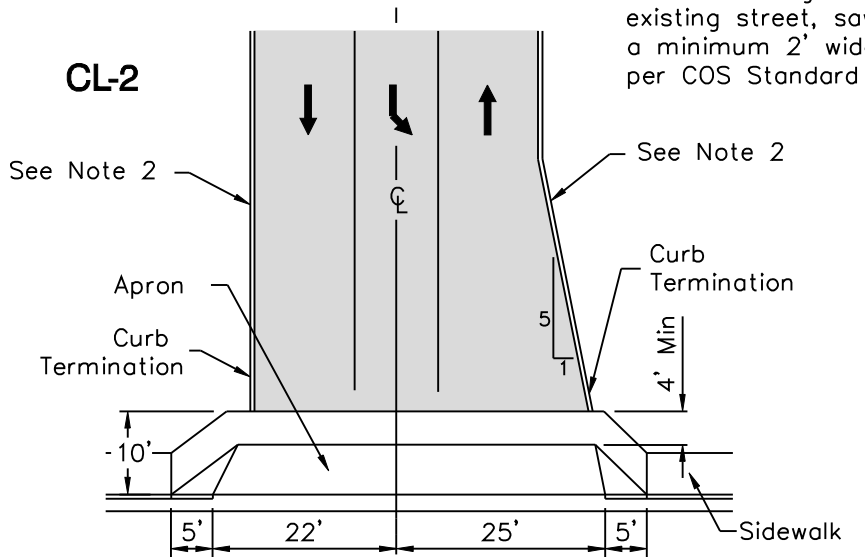
1. Sidewalk shall conform to MAG Standard Detail 230 and C.O.S. Sec. 340.
2. Curb as called for on plans.
3. Curb & Gutter shall conform to MAG Standard Detail 220. Single Curb shall conform to MAG Standard Detail 222.
4. Driveway Entrances shall conform to COS Standard Detail 2250 except for modifications as noted. Driveways shall be 2–1/2" A.C. A–19 on 6" A.B.C. unless otherwise noted. See plans for driveway limits.
5. Curb termination similar to MAG Std. Det. 222.
6. When installing new driveways along an existing street, sawcut, remove & replace a minimum 2' wide strip of A.C. pavement per COS Standard Detail 2200.

CL-1



TWO WAY

CL-2



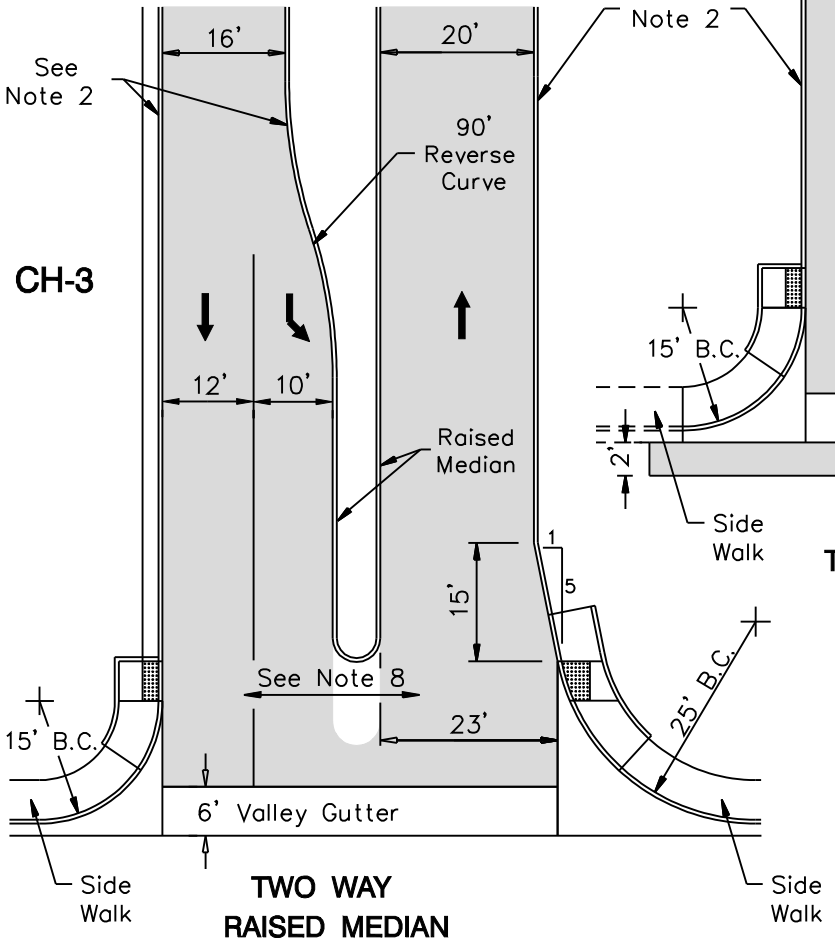
TWO WAY WITH
TWO EGRESS LANES

DETAIL NO. 2256	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	COMMERCIAL/INDUSTRIAL DRIVEWAYS-TYPE CL	DETAIL NO. 2256

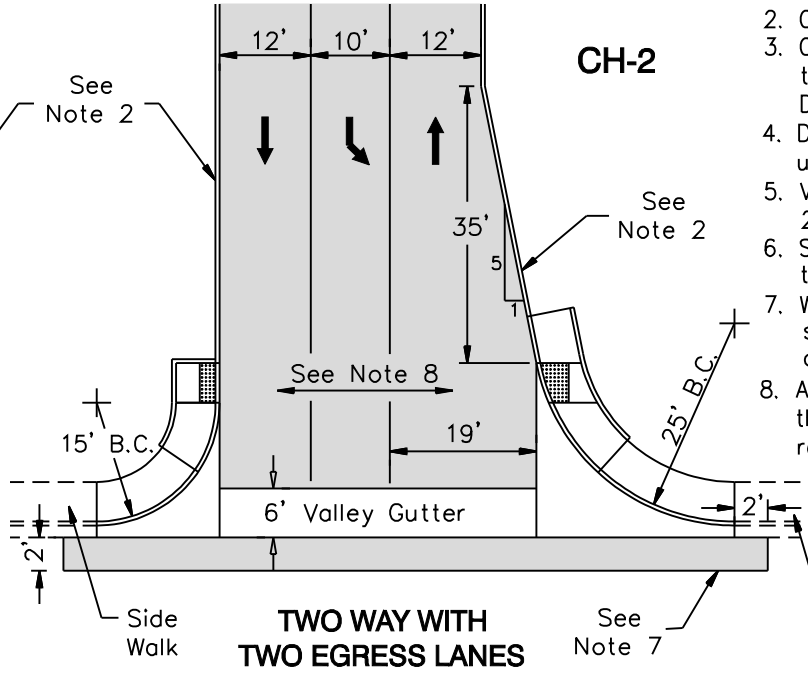
REVISED 5/17/06

CONSTRUCTION NOTES

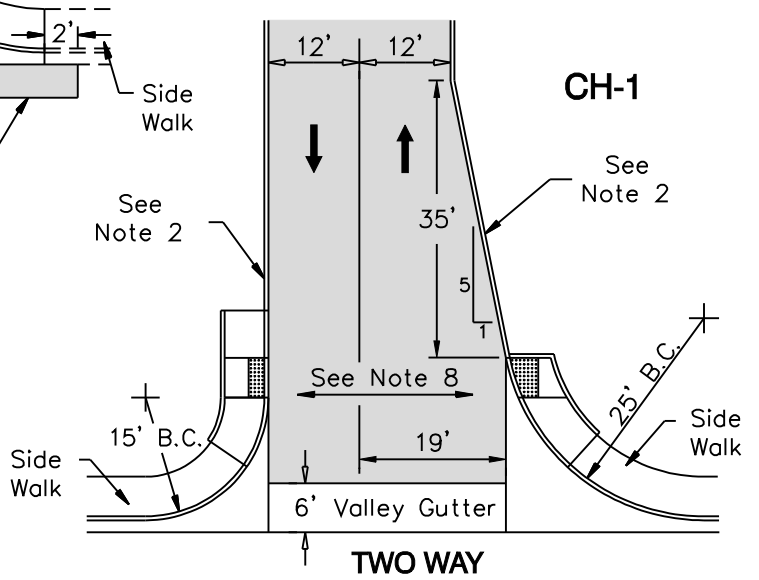
1. Sidewalk shall conform to MAG Standard Detail 230 and C.O.S. Sec. 340.
2. Curb as called for on plans.
3. Curb & Gutter shall conform to MAG Standard Detail 220. Single Curb shall conform to MAG Standard Detail 222.
4. Driveways shall be 2-1/2" A.C. A-19 on 6" A.B.C. unless otherwise noted. See plans for driveway limits.
5. Valley Gutter shall conform to COS Standard Detail 2240.
6. Sidewalk Ramps shall conform to COS Standard Details, Minimum 8" Thick.
7. When installing new driveways along an existing street, sawcut, remove & replace a minimum 2' wide strip of A.C. pavement per COS Standard Detail 2200.
8. All ramps and detectable warning shall be aligned in the direction of pedestrian travel and directed toward ramp on the opposite side of the street.



TWO WAY RAISED MEDIAN



TWO WAY WITH TWO EGRESS LANES



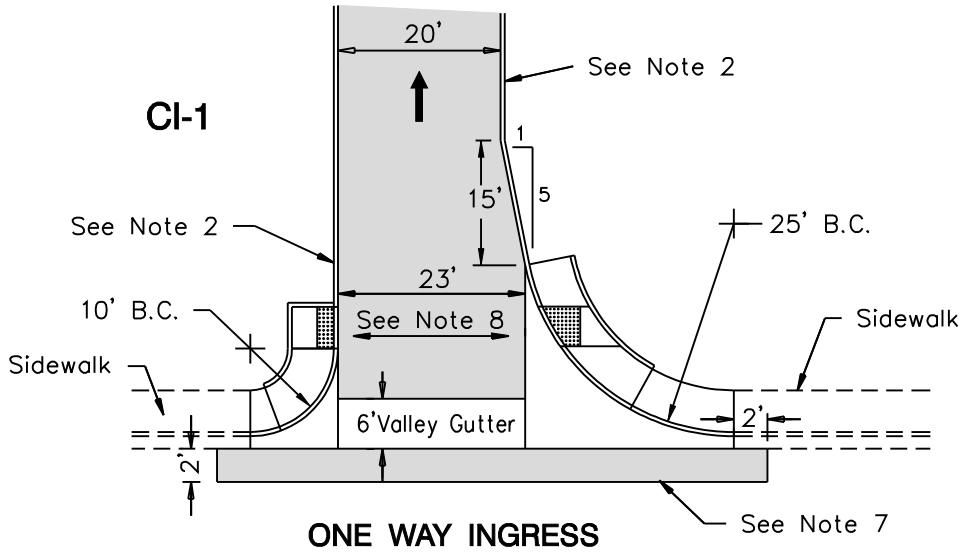
TWO WAY

DETAIL NO. 2257	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	COMMERCIAL/INDUSTRIAL DRIVEWAYS-TYPE CH	DETAIL NO. 2257

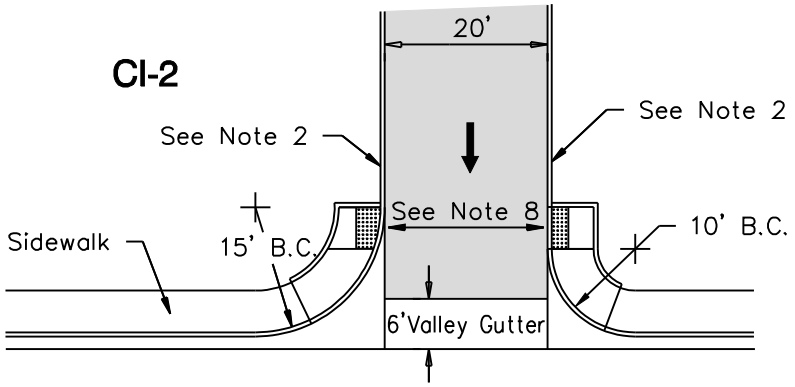
REVISED 5/17/06

CONSTRUCTION NOTES

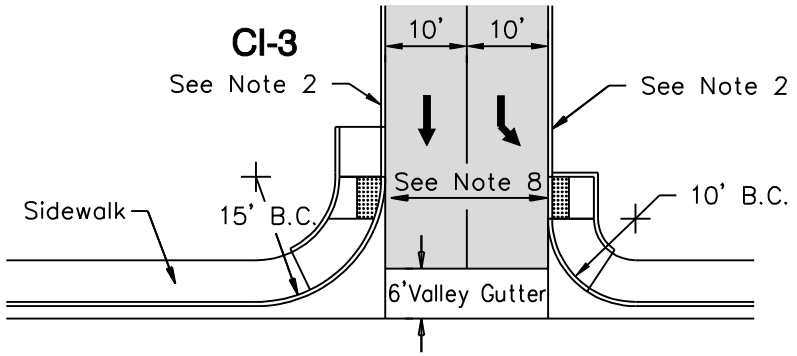
1. Sidewalk shall conform to MAG Standard Detail 230 and C.O.S. Sec. 340.
2. Curb as called for on plans.
3. Curb & Gutter shall conform to MAG Standard Detail 220. Single Curb shall conform to MAG Standard Detail 222.
4. Driveways shall be 2–1/2" A.C. A–19 on 6" A.B.C. unless otherwise noted. See plans for driveway limits.
5. Valley Gutter shall conform to COS Standard Detail 2240.
6. Sidewalk Ramps shall conform to COS Standard Details, Minimum 8" Thick.
7. When installing new driveways along an existing street, sawcut, remove & replace a minimum 2' wide strip of A.C. pavement per COS Standard Detail 2200.
8. All ramps and detectable warning shall be aligned in the direction of pedestrian travel and directed toward ramp on the opposite side of the street.



ONE WAY INGRESS

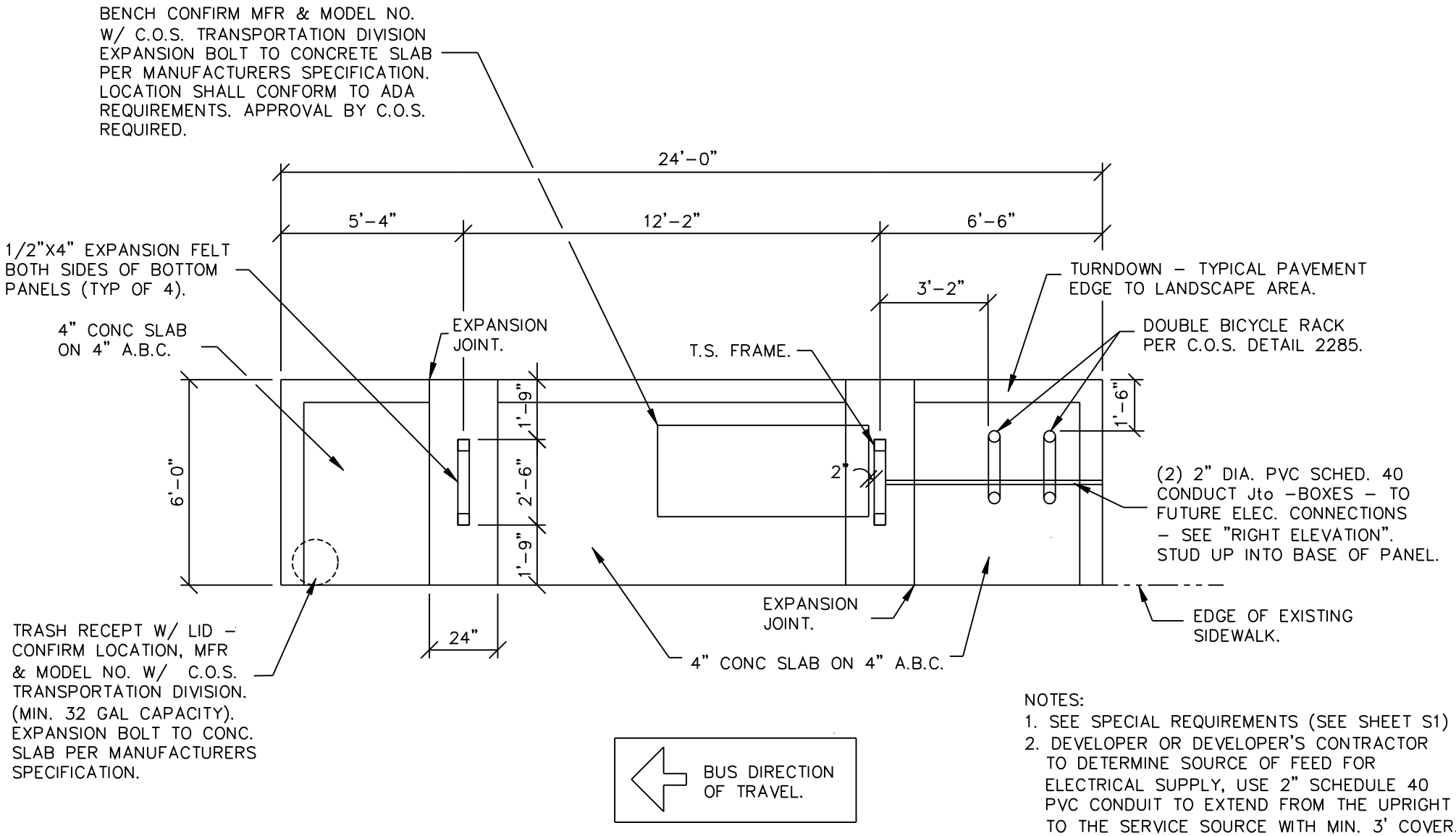


ONE WAY EGRESS



TWO LANES
ONE WAY EGRESS

DETAIL NO. 2258	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	COMMERCIAL/INDUSTRIAL DRIVEWAYS-TYPE CI	DETAIL NO. 2258



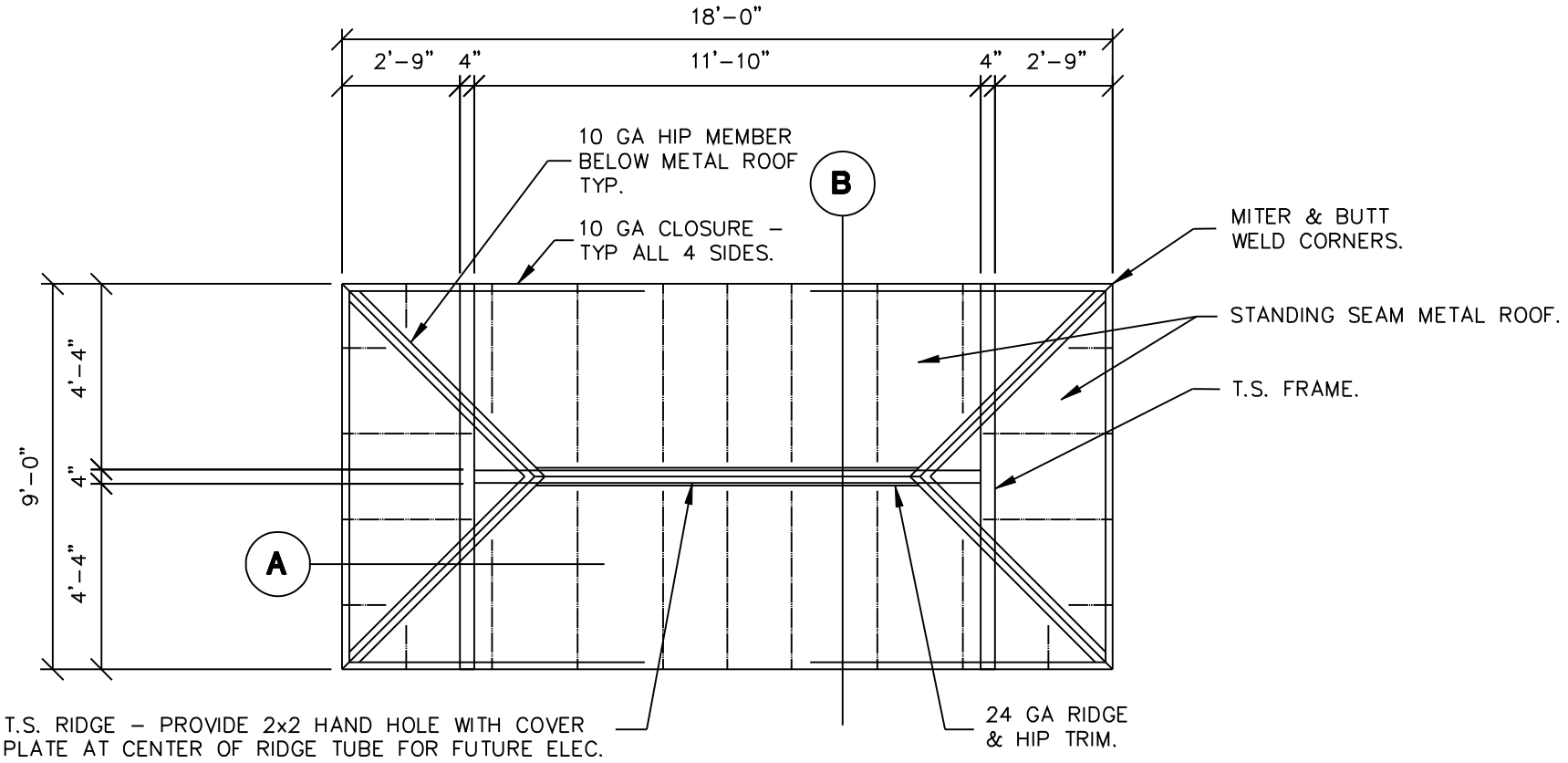
BUS SHELTER FOUNDATION PLAN

N.T.S.

NOTES:

1. SEE SPECIAL REQUIREMENTS (SEE SHEET S1)
2. DEVELOPER OR DEVELOPER'S CONTRACTOR TO DETERMINE SOURCE OF FEED FOR ELECTRICAL SUPPLY, USE 2" SCHEDULE 40 PVC CONDUIT TO EXTEND FROM THE UPRIGHT TO THE SERVICE SOURCE WITH MIN. 3' COVER. IF NO ELECTRICAL SUPPLY EXISTS, STUB CONDUIT 3' BEYOND SHELTER PAD AND CAP BOTH ENDS. CONTACT CITY STAFF TO COORDINATE DIRECTION OF STUB OUT.

DETAIL NO. 2265-1	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	BUS SHELTER	DETAIL NO. 2265-1

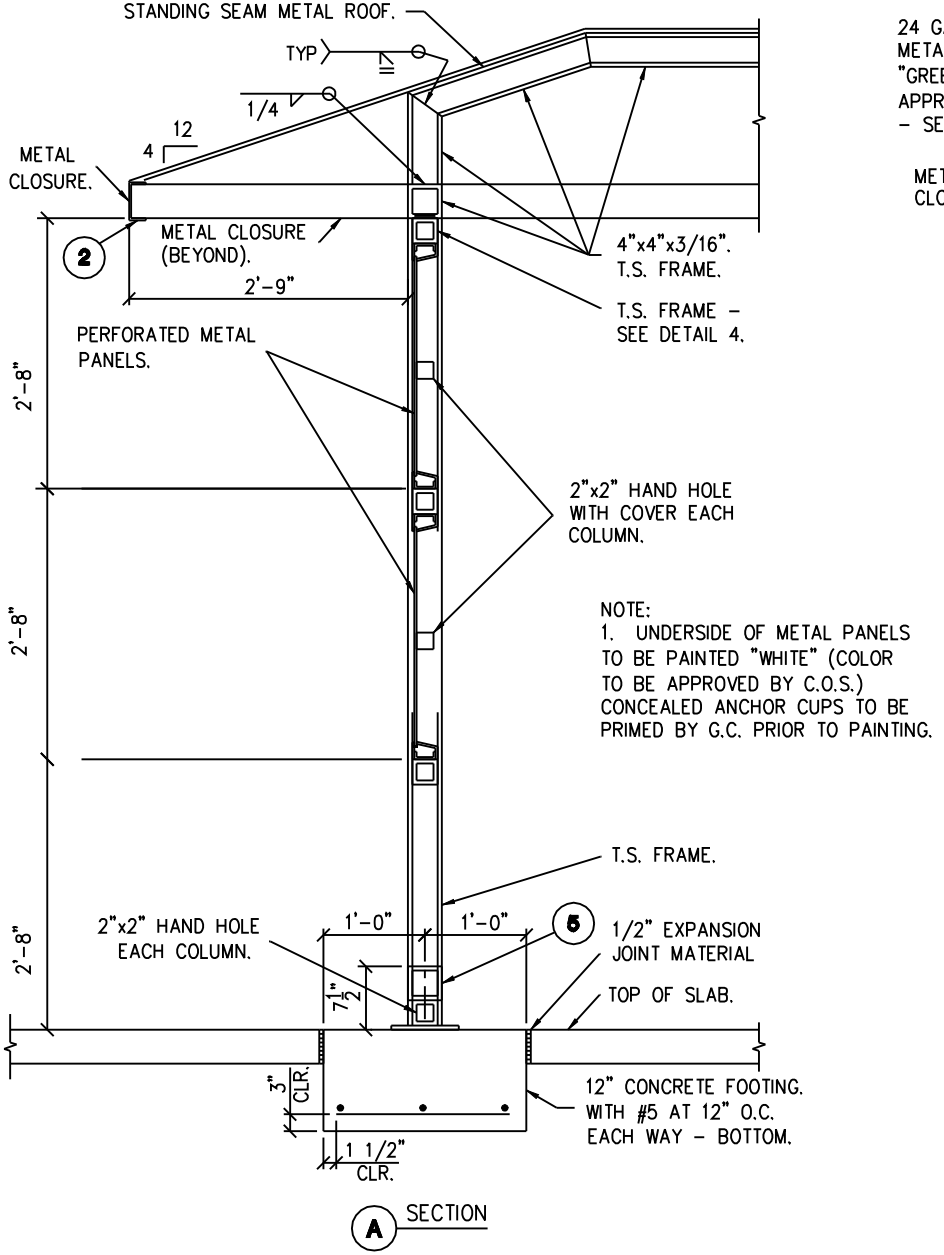


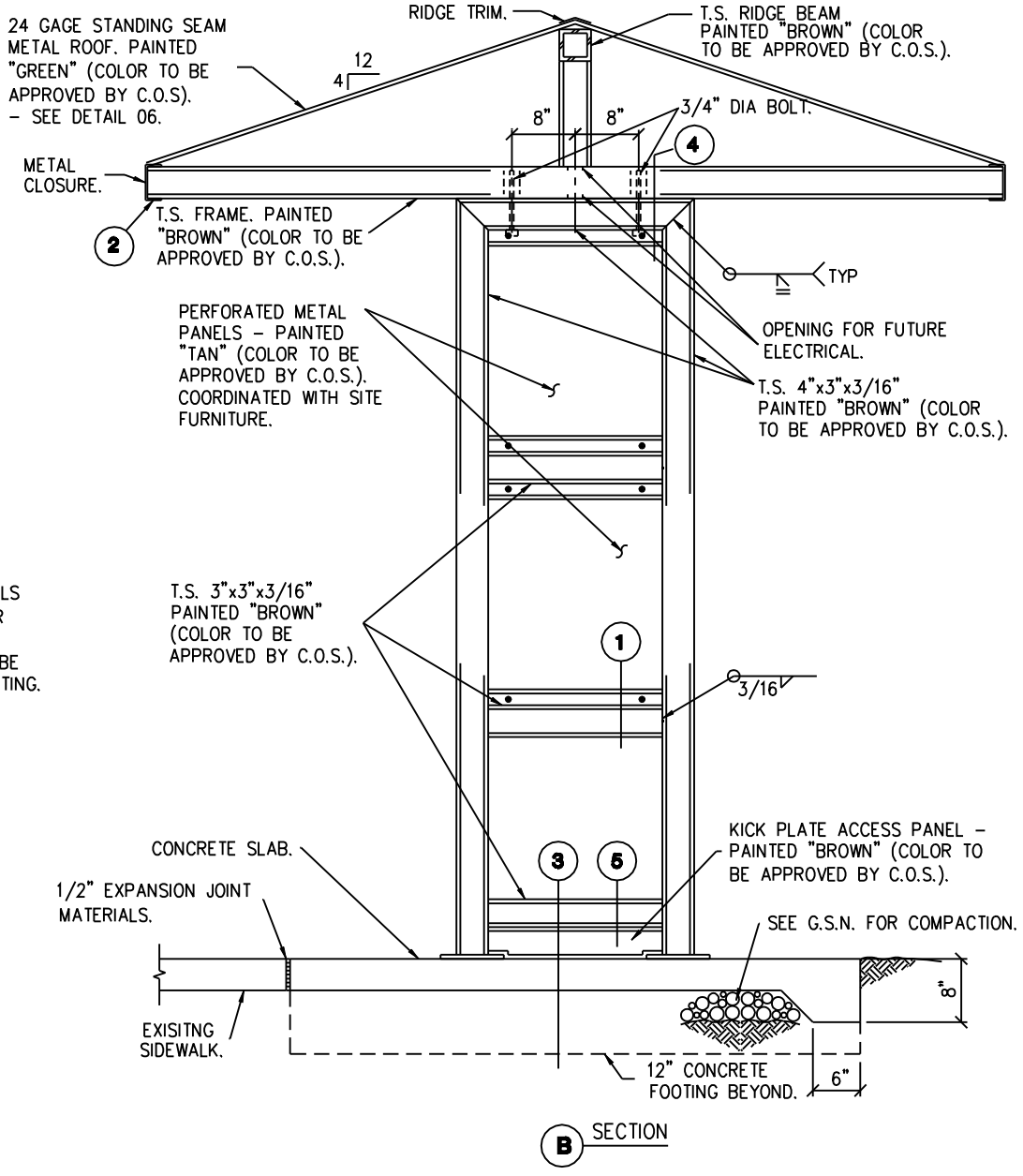
BUS SHELTER FRAMING PLAN

N.T.S.

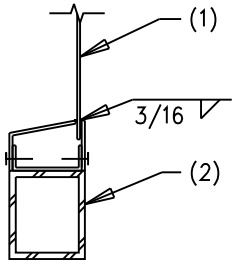
DETAIL NO. 2265-2	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	BUS SHELTER	DETAIL NO. 2265-2

REVISED 5/17/06





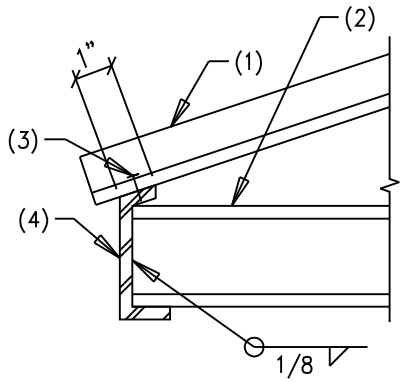
DETAIL NO. 2265-3	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	BUS SHELTER	DETAIL NO. 2265-3



NOTES:
1. PREFORATED PANEL. COORDINATE WITH SITE FURNITURE.
2. STEEL TUBE.

01 PANEL TO FRAME CONNECTION

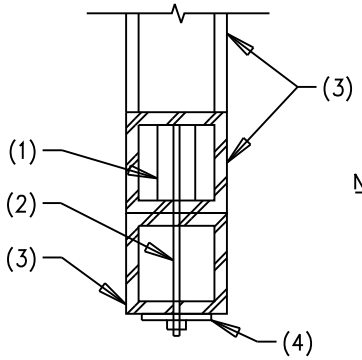
03-520 NO SCALE



NOTES:
1. STANDING SEAM METAL DECK.
2. STEEL TUBE.
3. BLACK OXIDE SCREW FASTENER AT 6" O.C. (MIN 4" AT EACH END).
4. CLOSURE PANEL.

02 TYPICAL CLOSURE

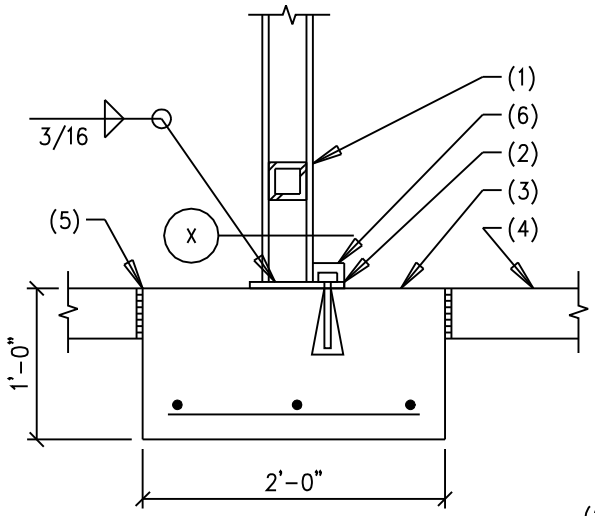
03-520 NO SCALE



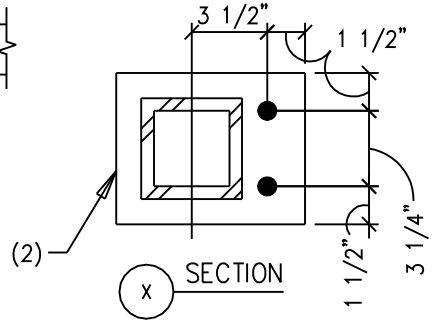
NOTES:
1. 3/4" THREAD INSERT.
2. 3/4" DIA. BOLT.
3. STEEL TUBE.
4. 3"x2"x1/4" PLATE WASHER.

04 CONNECTION AT ROOF STRUCTURE

03-520 NO SCALE



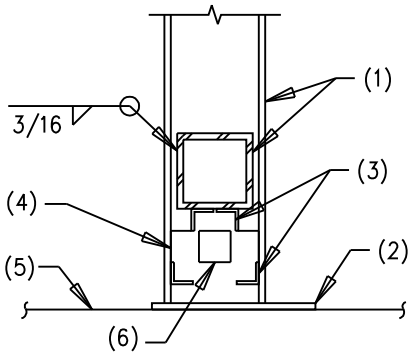
NOTES:
1. STEEL TUBE.
2. BASE PLATE 1/2"x6 1/4"x7 1/2" WITH 2 - 3/4" DIA. x 7" EXPANSION BOLT.
3. CONCRETE FOOTING.
4. CONCRETE SLAB.
5. EXPANSION JOINT MATERIAL.
6. BOLT COVER SEE DETAIL 08.



03 FOOTING AT T.S. FRAME

03-520 NO SCALE

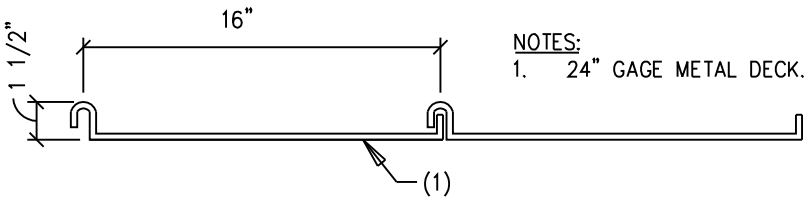
DETAIL NO. 2265-4	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	BUS SHELTER	DETAIL NO. 2265-4



NOTES:
1. STEEL TUBE.
2. BASE PLATE – SEE DETAIL 03.
3. 1"x1" ANGLE.
4. 16 GAGE PANEL.
5. TOP OF SLAB.
6. 2"x2" HAND HOLE AT TUBE FRAME BEYOND – SEE SECTION A FOR LOCATIONS.

05 KICK PLATE ACCESS PANEL

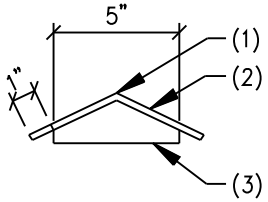
03-520 NO SCALE



NOTES:
1. 24" GAGE METAL DECK.

06 STANDING SEAM METAL DECK

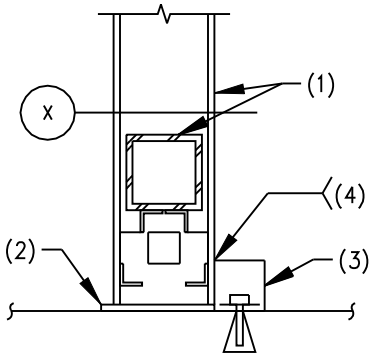
03-520 NO SCALE



NOTES:
1. Sy = .058
   Ix = .0219
   Fy = 50 KSI.
2. 10 GAGE HIP MEMBER BELOW METAL ROOF – TYP.
3. 22 GAGE METAL SNAP COVER PLATE – CREASE MIDDLE; TRIM EDGES TO FIT END CONDITIONS.

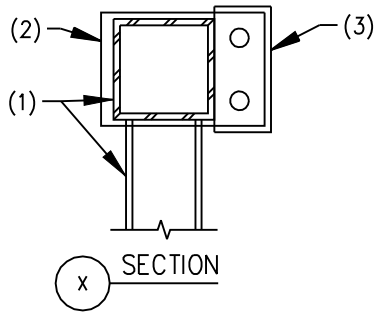
07 METAL COVER PLATE

03-520 NO SCALE



NOTES:
1. STEEL TUBE.
2. BASE PLATE.
3. 16 GAGE METAL COVER.
4. TACK WELD.

NOTE:
FOR INFORMATION SHOWN BUT NOT NOTED SEE DET 05.



08 BOLT COVER

03-520 NO SCALE

DETAIL NO. 2265-5	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	BUS SHELTER	DETAIL NO. 2265-5

GENERAL STRUCTURAL NOTES

BUILDING CODE:

2003 EDITION OF THE UNIFORM BUILDING CODE, WITH CITY OF SCOTTSDALE AMENDMENTS.

LOADS:

LATERAL:
WIND LOAD = 90 MPH WIND SPEED, EXPOSURE C.
SEISMIC ZONE 2B (Z = 0.075)

FOUNDATIONS:

COMPACT SUB GRADE AND BASE MATERIAL TO 95% OF THE ASTM D698 MAXIMUM DRY DENSITY.

CONCRETE:

MINIMUM 28 DAY STRENGTH 3,000 PSI

ALL CAST-IN-PLACE CONCRETE CONSTRUCTION SHALL CONFORM TO THE LATEST EDITION OF THE ACI. FOR CONCRETE WITHOUT PLASTICIZER, MAXIMUM SLUMP 4 1/2" AT POINT OF PLACEMENT U.N.O. IF PLASTICIZER IS USED, A HIGHER FINAL SLUMP MAY BE ALLOWED UPON STRUCTURAL ENGINEER'S APPROVAL.

REINFORCING:

ALL REINFORCING PER CRSI SPECIFICATIONS AND HANDBOOK. ASTM A615 (Fy = 60 KSI/GRADE 60) DEFORMED BARS FOR ALL BARS.

ALL REINFORCING SHALL BE CHAIRED TO ENSURE PROPER CLEARANCES. SUPPORT OF FOUNDATION REINFORCING MUST PROVIDE ISOLATION FROM MOISTURE CORROSION BY USE OF A PLASTIC OR CONCRETE CHAIR. DUCT-TAPE COVERED REINFORCING IS NOT AN ACCEPTABLE CHAIR.

ALL DIMENSIONS REFERENCED IN DRAWINGS AS "CLEAR" SHALL BE FROM FACE OF STRUCTURE TO EDGE OF REINFORCING, AND SHALL NOT BE LESS THAN STATED, NOR GREATER THAN "CLEAR" DIMENSION PLUS 3/8". ALL OTHERS SHALL BE PLUS OR MINUS 1/4" TYPICAL UNLESS NOTED OTHERWISE.

STRUCTURAL STEEL:

ALL CONSTRUCTION PER LATEST AISC HANDBOOK. ALL TUBE STEEL SHALL BE ASTM A500(Fy=46 KSI). ALL MISCELLANEOUS STEEL UNLESS NOTED OTHERWISE SHALL BE ASTM A36 (Fy = 36 KSI).

UNLESS NOTED OTHERWISE, ALL WELDS PER LATEST EDITION OF THE AWS STANDARDS. ALL WELDING SHALL BE PERFORMED BY WELDERS HOLDING VALID CERTIFICATES AND HAVING CURRENT EXPERIENCE IN THE TYPE OF WELD SHOWN ON THE DRAWINGS OR NOTES. CERTIFICATES SHALL BE THOSE ISSUED BY AN ACCEPTED

STRUCTURAL STEEL: CONT'D

TESTING AGENCY. ALL WELDING DONE BY E70 SERIES LOW HYDROGEN RODS UNLESS NOTED OTHERWISE. FOR GRADE 60 REINFORCING BARS, USE E90 SERIES. THESE DRAWINGS DO NOT DISTINGUISH BETWEEN SHOP AND FIELD WELDS; THE CONTRACTOR MAY SHOP WELD OR FIELD WELD AT THEIR DISCRETION. SHOP WELDS AND FIELD WELDS SHALL BE SHOWN ON THE SHOP DRAWINGS SUBMITTED FOR REVIEW.

STEEL DECKING:

ALL STANDING SEAM DECK SHALL CARRY A U.L. 90 UPLIFT RATING. INSTALLATION SHALL CONFORM TO STANDARDS SET FORTH IN THE ARCHITECTURAL SHEET METAL MANUAL PUBLISHED BY SMACNA.

WELDERS EXPERIENCED IN LIGHT GAGE STEEL DECK WORK SHALL PERFORM ALL WELDING. DECK WELDING MAY BE ACHIEVED WITH E60 SERIES NON LOW HYDROGEN RODS OR E70 SERIES LOW HYDROGEN RODS.

SCREWS WHERE INDICATED SHALL BE #12-24 TRAXX PER ICBO 3056 OR APPROVED EQUIVALENT.

SHOP DRAWINGS:

SHOP DRAWINGS SHALL BE SUBMITTED FOR ALL STRUCTURAL ITEMS.

THE CONTRACTOR SHALL REVIEW ALL SHOP DRAWINGS PRIOR TO SUBMITTAL. ITEMS NOT IN ACCORDANCE WITH CONTRACT DOCUMENTS SHALL BE FLAGGED UPON CONTRACTOR'S REVIEW.

MANUFACTURER OR FABRICATOR SHALL CLOUD ANY CHANGES, SUBSTITUTIONS, OR DEVIATIONS FROM CONTRACT DOCUMENTS. ANY OF THE AFOREMENTIONED WHICH ARE NOT CLOUDED OR FLAGGED BY SUBMITTING PARTIES, SHALL NOT BE CONSIDERED APPROVED AFTER ENGINEER'S REVIEW, UNLESS NOTED ACCORDINGLY.

THE ENGINEER HAS THE RIGHT TO APPROVE OR DISAPPROVE ANY CHANGES TO CONTRACT DOCUMENTS AT ANYTIME BEFORE OR AFTER SHOP DRAWING REVIEW.

THE SHOP DRAWINGS DO NOT REPLACE THE CONTRACT DOCUMENTS. ITEMS OMITTED OR SHOWN INCORRECTLY AND ARE NOT FLAGGED BY THE STRUCTURAL ENGINEER OR ARCHITECT SHALL NOT BE CONSIDERED CHANGES TO CONTRACT DOCUMENTS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ENSURE ITEMS ARE CONSTRUCTED TO CONTRACT DOCUMENTS. THE ADEQUACY OF ENGINEERING DESIGNS AND LAYOUT PERFORMED BY OTHERS RESTS WITH THE DESIGNING OR SUBMITTING AUTHORITY.

SHOP DRAWINGS: CONT'D

REVIEWING IS INTENDED ONLY AS AN AID TO THE CONTRACTOR IN OBTAINING CORRECT SHOP DRAWINGS. RESPONSIBILITY FOR CORRECTNESS SHALL REST WITH THE CONTRACTOR.

SPECIAL REQUIREMENTS

1. AN ARTIST-DESIGNED SHELTER MAY BE SUBSTITUTED FOR STANDARD SHELTER BY APPROVAL OF THE CITY OF SCOTTSDALE TRANSIT SECTION. HOWEVER, IT MUST INCORPORATE ALL THE FUNCTIONAL ELEMENTS INCLUDED IN THE STANDARD SHELTER. SEE TRANSIT & DESIGN REVIEW STAFF FOR DETAILS.

2. STANDARD BUS STOP SIGN LOCATION, NEW OR RELOCATED SIGNS SHALL BE APPROVED BY THE TRAFFIC/TRANSIT STAFF.

3. ADDITIONAL REQUIREMENTS MAY INCLUDE:
A) LEANING RAIL.
B) LED REAL TIME BUS INFORMATION SIGN.
C) BUS ROUTE/TRAFFIC INFORMATION KIOSKS.

D) PEDESTRIAN RAILING AROUND THE BACK OF SHELTER ADJACENT TO STEEP SLOPES OR DROP-OFFS.

4. CITY OF SCOTTSDALE TRANSIT BUS SHELTERS SHALL BE PROVIDED WITH A GROUNDING SYSTEM THAT MAY CONSIST OF ONE OF THE FOLLOWING METHODS:
A) 25 FEET OF #4 STANDARD COPPER (UNINSULATED) INSTALLED IN THE BASE OF ONE OF THE UPRIGHT FOUNDATIONS. THE GROUNDING CONDUCTOR WILL EXTEND OUT OF THE POURED CONCRETE FOUNDATION WITH A LENGTH NOT TO EXCEED 3 FEET. THE GROUNDING CONDUCTOR WILL BE WRAPPED IN A CLOCKWISE ROTATION, ONE WRAP, AROUND ON THE THE UPRIGHT ANCHOR BOLTS. A FLAT FENDER WASHER WILL BE INSTALLED ON TOP OF THE CONDUCTOR WITH THE ANCHOR BOLT NUT ON TOP OF THE FLAT WASHER AND SECURED.
B) A SECOND METHOD WILL CONSIST OF A 5/8"x 8'-0" GROUND ROD DRIVEN IN THE ELECTRICAL PULLBOX ADJACENT TO THE BUS SHELTER. A GROUND ROD TERMINAL NUT (ACORN NUT) WILL BE INSTALLED ON TOP OF THE GROUND ROD SECURING A #8 AWG BARE SOLID COPPER WIRE. THE GROUND WIRE WILL BE INSTALLED FROM THE JUNCTION BOX, UNBROKEN AND UNSPLICED, TO THE BUS SHELTER UPRIGHT WHERE IT WILL BE TERMINATED. A SET-SCREW TERMINAL LUG WILL BE FASTENED TO THE STRUCTURE UPRIGHT UNDER THE BOTTOM KICKPANEL. THE AREA UNDER THE TERMINAL LUG WILL BE CLEANED OF ALL RUST, SCALE AND PAINT. THE #8 BARE BOND CONDUCTOR WILL BE TERMINATED IN THE SET-SCREW TERMINAL LUG.

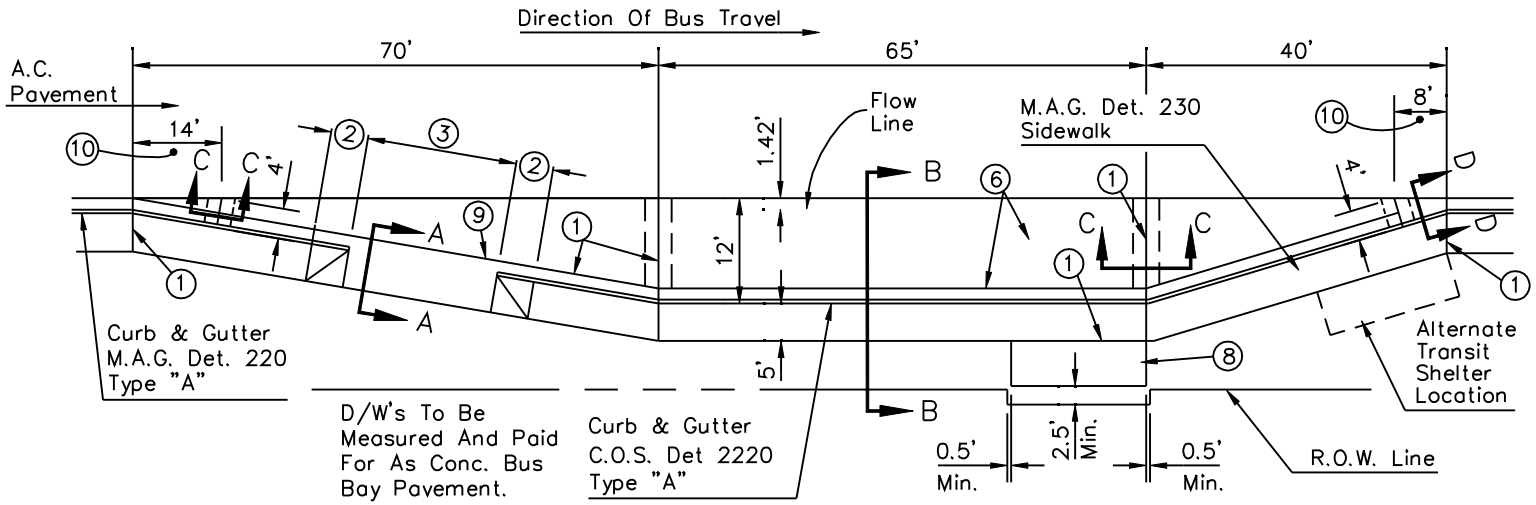
BOTH GROUNDING METHODS WILL BE DONE IN ACCORDANCE WITH ARTICLE 250 OF NATIONAL ELECTRICAL CODE.

ABBREVIATIONS

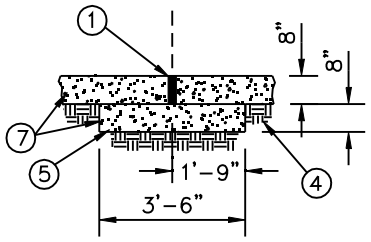
NOTE: ABBREVIATIONS MAY OR MAY NOT HAVE PERIODS, BUT SHALL BE READ AS SAME.

A.B.	ANCHOR BOLT
A.B.C.	AGGREGATE BASE COURSE
ACI	AMERICAN CONCRETE INSTITUTE
A/C	AIR CONDITIONER
A.F.F.	ABOVE FINISHED FLOOR
AISC	AMERICAN INSTITUTE OF STEEL CONSTRUCTION
AISI	AMERICAN IRON AND STEEL INSTITUTE
AITC	AMERICAN INSTITUTE OF TIMBER CONSTRUCTION
ALT.	ALTERNATE
ANSI	AMERICAN NATIONAL STANDARDS INSTITUTE
APA	AMERICAN PLYWOOD ASSOCIATION
ARCH'L	ARCHITECTURAL
ASTM	AMERICAN SOCIETY FOR TESTING AND MATERIALS
AWS	AMERICAN WELDING SOCIETY
@	AT (MEASUREMENT)
BM	BEAM
B.F.F	BELOW FINISHED FLOOR
BLK	BLOCK
B.O.B.	BOTTOM OF BEAM
B.O.D.	BOTTOM OF DECK
B.O.F.	BOTTOM OF FOOTING
BRG	BEARING
C	CAMBER
C.C.	CENTERLINE TO CENTERLINE
C.G.	CENTER OF GRAVITY
C.I.P.	CAST IN PLACE
C.L.	CENTERLINE
C.L.B.	CENTERLINE OF BEAM
C.L.C.	CENTERLINE OF COLUMN
C.L.F.	CENTERLINE OF FOOTING
C.L.W.	CENTERLINE OF WALL
CLR	CLEAR
CONC	CONCRETE
CONC C.J.	CONCRETE CONTROL JOINT
CONC S.J.	CONCRETE SAWCUT JOINT
C.M.U.	CONCRETE MASONRY UNIT
CONN	CONNECTION
CONT	CONTINUOUS
C.O.S.	CITY OF SCOTTSDALE
CRSI	CONCRETE REINFORCING STEEL INSTITUTE
DL	DEAD LOAD
ø OR DIA	DIAMETER
DN	DOWN
DWG(S)	DRAWING(S)
E.C.	END TO CENTERLINE
E.E.	END TO END
E.O.S.	EDGE OF SLAB
EQ	EQUAL
EQUIP	EQUIPMENT
EXP. BOLT (E.B.)	EXPANSION BOLT
EXP. JT (E.J.)	EXPANSION JOINT
E.W.	EACH WAY
F.F.	FINISHED FLOOR
F.O.M.	FACE OF MEMBER
F.O.S.	FACE OF STEEL
F.O.W.	FACE OF WALL
GA	GAGE (UNIT OF MEASUREMENT)
GALV	GALVANIZED
G.S.N.	GENERAL STRUCTURAL NOTES
GLB (GLULAM)	GLUED-LAMINATED BEAM
HORIZ	HORIZONTAL REINFORCING
IBC	INTERNATIONAL BUILDING CODE
ICBO	INTERNATIONAL CONFERENCE OF BUILDING OFFICIALS
I.F.W.	INSIDE FACE OF WALL
I.O.D.	INTERPRETATION OF DRAWINGS
K(KIP)	1000 POUNDS
KLF	KIPS PER LINEAR FOOT
LBS (#)	POUNDS
LGS	LIGHT GAGE STEEL
LGSEA	LIGHT GAGE STEEL ENGINEERS ASSOCIATION
L.O.D.	LOCATION OF DETAILS
LL	LIVE LOAD
LLH	LONG LEG HORIZONTAL
LLV	LONG LEG VERTICAL
MAS	MASONRY
MAS C.J.	MASONRY CONTROL JOINT
MAX	MAXIMUM
MBMA	METAL BUILDING MANUFACTURERS ASSOCIATION
MECH'L	MECHANICAL
MFR('S)	MANUFACTURER('S)
MIN	MINIMUM
N/A	NOT APPLICABLE
N.T.S.	NOT TO SCALE
O.C.	ON CENTER
O.F.W.	OUTSIDE FACE OF WALL
OPP	OPPOSITE
OSHA	OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION
PCI	PRECAST/PRESTRESSED CONCRETE INSTITUTE
P.C.	PRECAST CONCRETE
PLF	POUNDS PER LINEAR FOOT
±	PLUS OR MINUS
PREFAB	PREFABRICATED
PSF	POUNDS PER SQUARE FOOT
PSI	POUNDS PER SQUARE INCH
PTI	POST-TENSIONING INSTITUTE
REINF	REINFORCING
SDI	STEEL DECK INSTITUTE
SLH	SHORT LEG HORIZONTAL
SLV	SHORT LEG VERTICAL
SJI	STEEL JOIST INSTITUTE
SIM	SIMILAR
SQ.	SQUARE
SSMA	STEEL STUD MANUFACTURERS ASSOCIATION
STD	STANDARD
STL	STEEL
TL	TOTAL LOAD
T.O.B.	TOP OF BEAM
T.O.C.T.	TOP OF CONCRETE TOPPING
T.O.D.	TOP OF DECK
T.O.F.	TOP OF FOOTING
T.O.L.	TOP OF LEDGER
T.O.M.	TOP OF MASONRY
T.O.P.	TOP OF PLATE
T.O.P.C.	TOP OF PRECAST CONCRETE
T.O.S.	TOP OF STEEL
T.O.W.	TOP OF WALL
TPI	TRUSS PLATE INSTITUTE
TYP	TYPICAL
T&G	TONGUE AND GROOVE
UBC	UNIFORM BUILDING CODE
U.N.O.	UNLESS NOTED OTHERWISE
VERT	VERTICAL REINFORCING
WCLA	WEST COAST LUMBER ASSOCIATION
WCLIB	WEST COAST LUMBER INSPECTION BUREAU
W.W.F.	WELDED WIRE FABRIC
WWPA	WESTERN WOOD PRODUCTS ASSOCIATION
W/	WITH
W/C	WATER TO CEMENT RATIO
W/O	WITHOUT

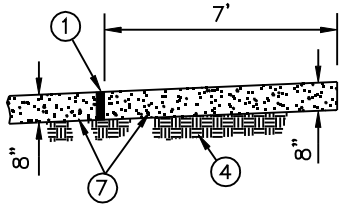
DETAIL NO.		APPROVED BY:		DETAIL NO.
2265-7	City of Scottsdale Standard Details	Scottsdale Standards & Specifications Committee	BUS SHELTER	2265-7



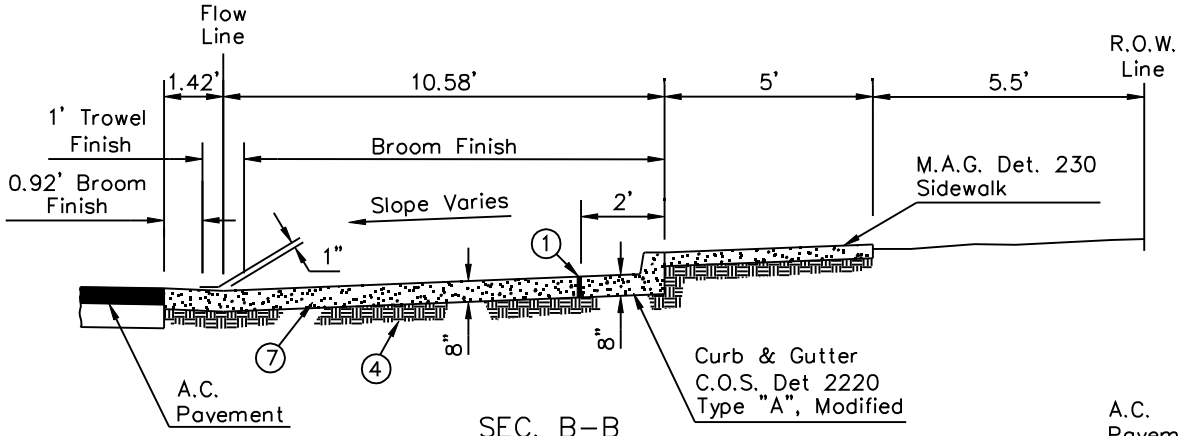
PLAN VIEW



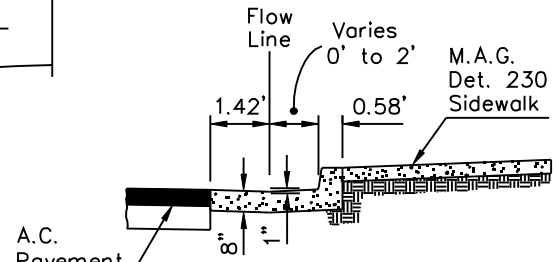
SEC. C–C



SEC. A–A



SEC. B–B

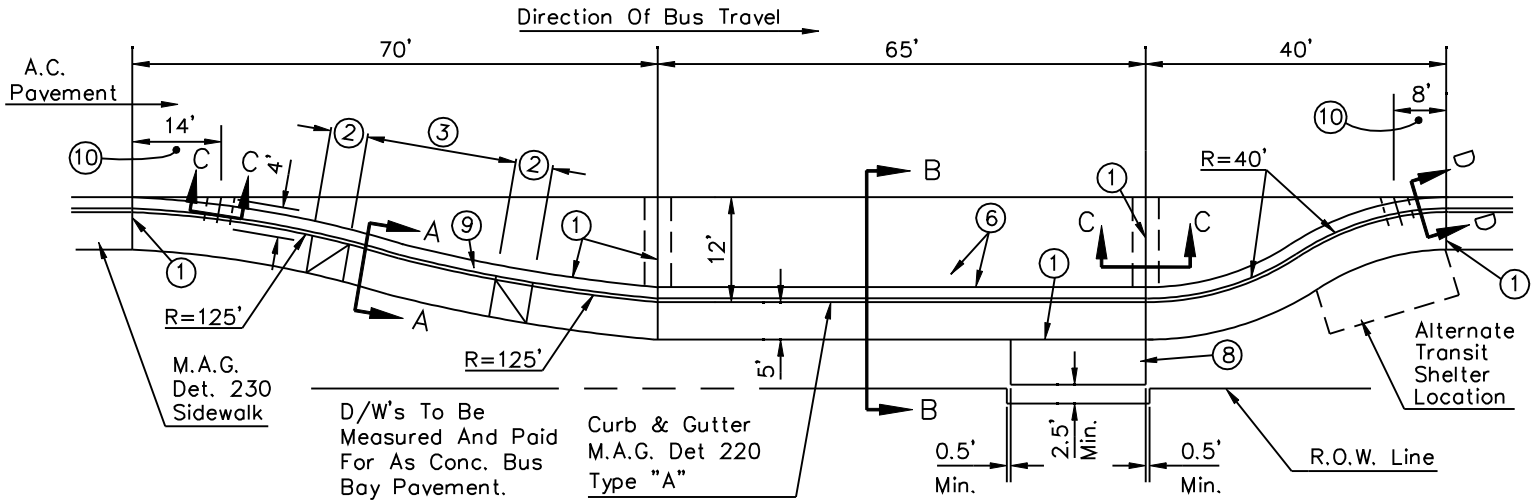


SEC. D–D

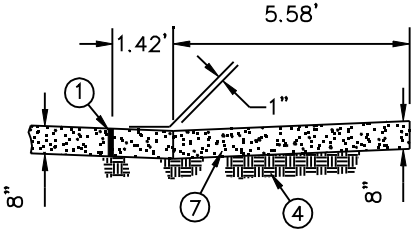
1. 1/2" BITUMINOUS PREFORMED EXPANSION JOINT FILLER, A.S.T.M. D–1751 PER MAG SEC. 729, INSTALL PER COS SEC. 340.3.
2. TRANSITION CURB FACE FROM 6" TO 0" HEIGHT IN 5'
3. D/W WIDTHS & LOCATIONS VARY, SEE PLANS. MAINTAIN 10' MIN. CLEARANCE BETWEEN DRIVEWAYS AND SHELTER PAD. WHEN DRIVE–WAY WIDTH EXCEEDS 22', PROVIDE A CONTRACTION JOINT ON D/W ℄.
4. SUBGRADE PREPARATION PER MAG SECTION 301.
5. CONCRETE PAD TO BE POURED SEPARATELY FROM CONCRETE BUS BAY PAVEMENT.
6. CONTRACTION JOINTS IN THE BUS BAY PAVEMENT SHALL MATCH THOSE IN THE CURB.
7. CONCRETE SHALL BE CLASS "A" PER MAG SECTION 725
8. TRANSIT SHELTER PAD, PER COS STD DETAIL 2265.
9. NEW CONCRETE D/W TO BE POURED WITH BUS BAY PAVEMENT.
10. CURB & GUTTER TO BUS BAY PAVEMENT TRANSITION.

REVISED 5/17/06

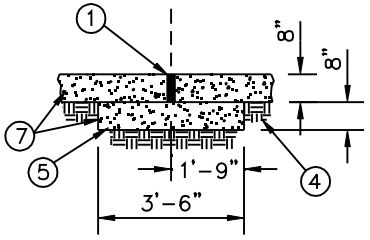
DETAIL NO. 2266-1	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	CLOSED END BUS BAY - TYPE "A'	DETAIL NO. 2266-1



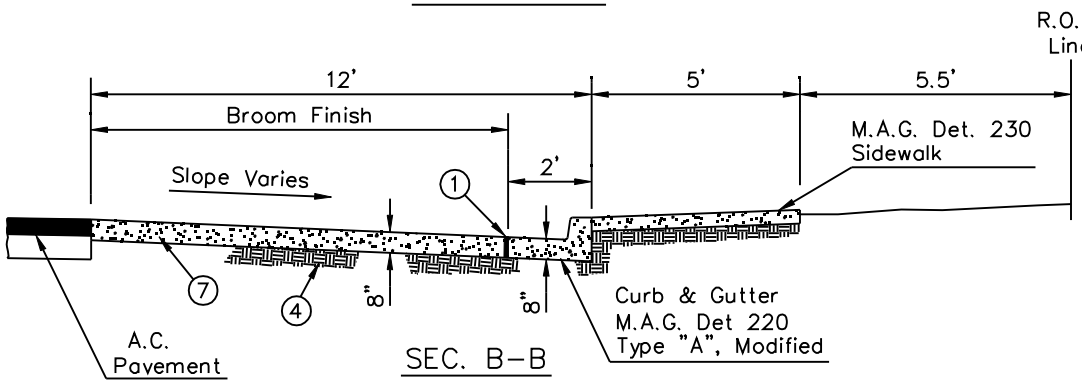
PLAN VIEW



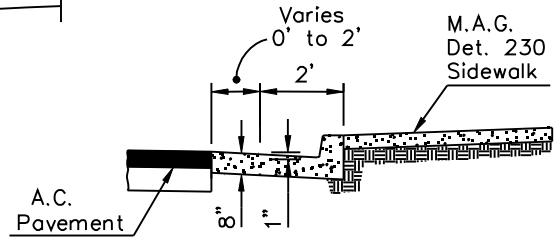
SEC. A–A



SEC. C–C



SEC. B–B

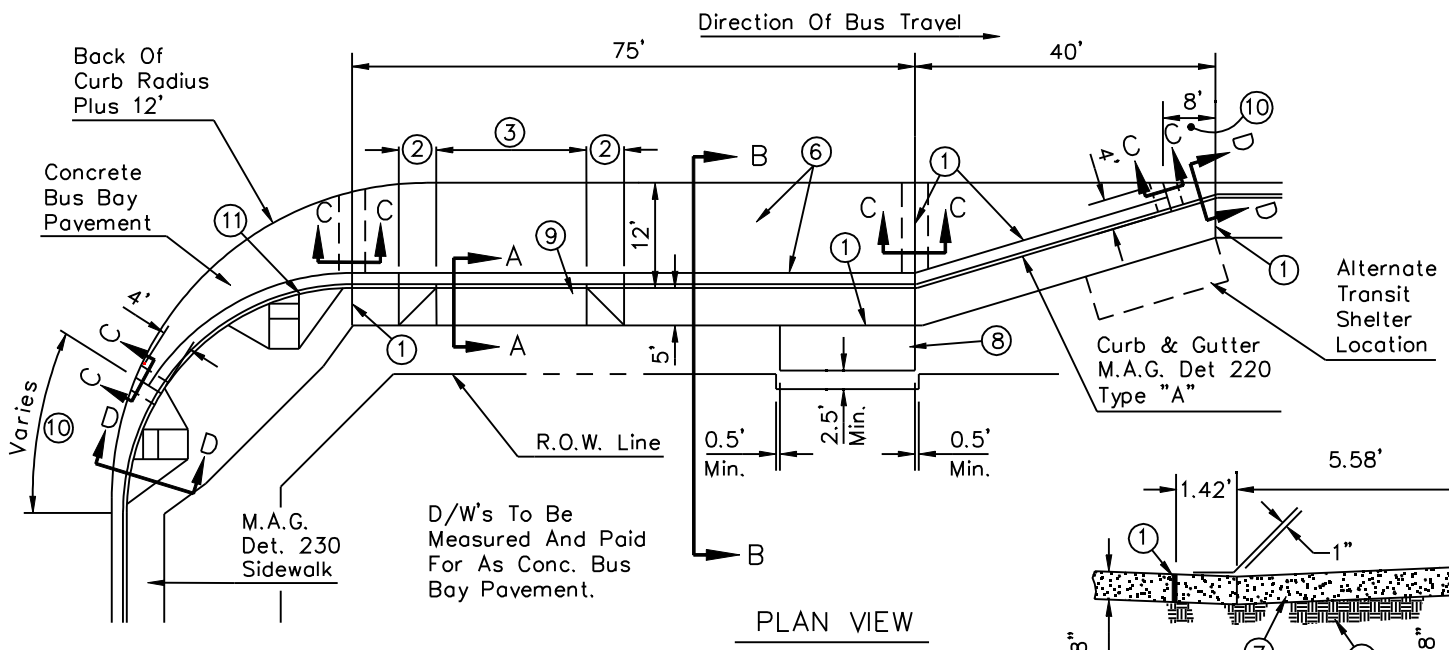


SEC. D–D

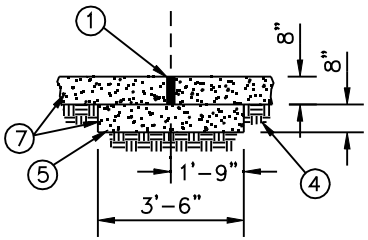
1. 1/2" BITUMINOUS PREFORMED EXPANSION JOINT FILLER, A.S.T.M. D–1751 PER MAG SECTION 729.
2. TRANSITION CURB FACE FROM 6" TO 0" HEIGHT IN 5'
3. D/W WIDTHS & LOCATIONS VARY, SEE PLANS. MAINTAIN 10' MIN. CLEARANCE BETWEEN DRIVEWAYS AND SHELTER PAD. WHEN DRIVE–WAY WIDTH EXCEEDS 22', PROVIDE A CONTRACTION JOINT ON D/W ℄.
4. SUBGRADE PREPARATION PER MAG SECTION 301.
5. CONCRETE PAD TO BE POURED SEPARATELY FROM CONCRETE BUS BAY PAVEMENT.
6. CONTRACTION JOINTS IN THE BUS BAY PAVEMENT SHALL MATCH THOSE IN THE CURB.
7. CONCRETE SHALL BE CLASS "A" PER MAG SECTION 725
8. TRANSIT SHELTER PAD, PER COS STD DETAIL 2265.
9. NEW CONCRETE D/W TO BE POURED WITH BUS BAY PAVEMENT.
10. CURB & GUTTER TO BUS BAY PAVEMENT TRANSITION.

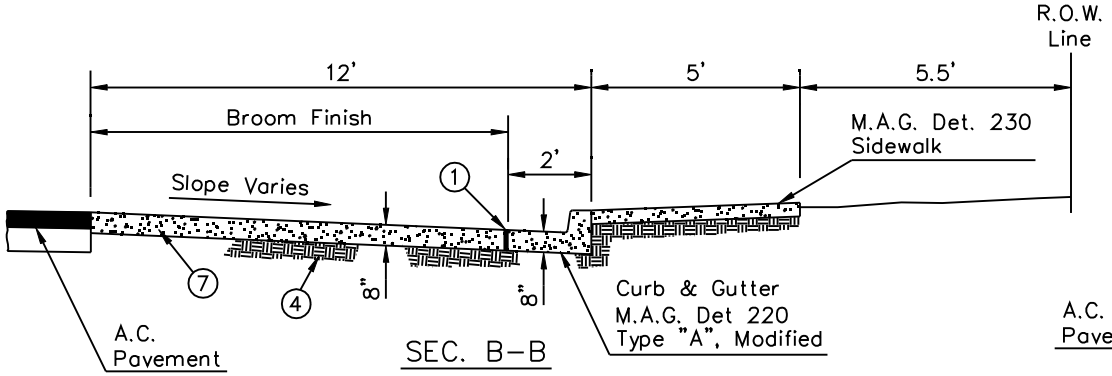
REVISED 5/17/06

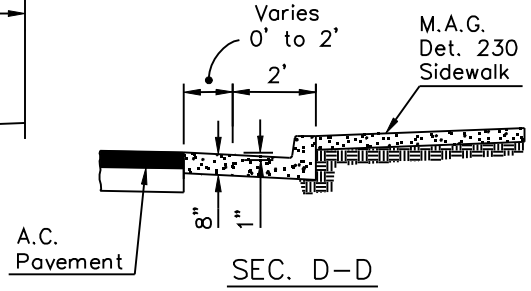
DETAIL NO. 2266-2	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	CLOSED END BUS BAY - TYPE "B"	DETAIL NO. 2266-2



1. 1/2" BITUMINOUS PREFORMED EXPANSION JOINT FILLER, A.S.T.M. D–1751 PER MAG SECTION 729.
2. TRANSITION CURB FACE FROM 6" TO 0" HEIGHT IN 5'
3. D/W WIDTHS & LOCATIONS VARY, SEE PLANS. MAINTAIN 10' MIN. CLEARANCE BETWEEN DRIVEWAYS AND SHELTER PAD. WHEN DRIVE–WAY WIDTH EXCEEDS 22', PROVIDE A CONTRACTION JOINT ON D/W ℄.
4. SUBGRADE PREPARATION PER MAG SECTION 301.
5. CONCRETE PAD TO BE POURED SEPARATELY FROM CONCRETE BUS BAY PAVEMENT.
6. CONTRACTION JOINTS IN THE BUS BAY PAVEMENT SHALL MATCH THOSE IN THE CURB.
7. CONCRETE SHALL BE CLASS "A" PER MAG SECTION 725
8. TRANSIT SHELTER PAD, PER COS STD DETAIL 2265.
9. NEW CONCRETE D/W TO BE POURED WITH BUS BAY PAVEMENT.
10. CURB & GUTTER TO BUS BAY PAVEMENT TRANSITION.
11. CURB RADIUS AS SHOWN ON THE PLANS



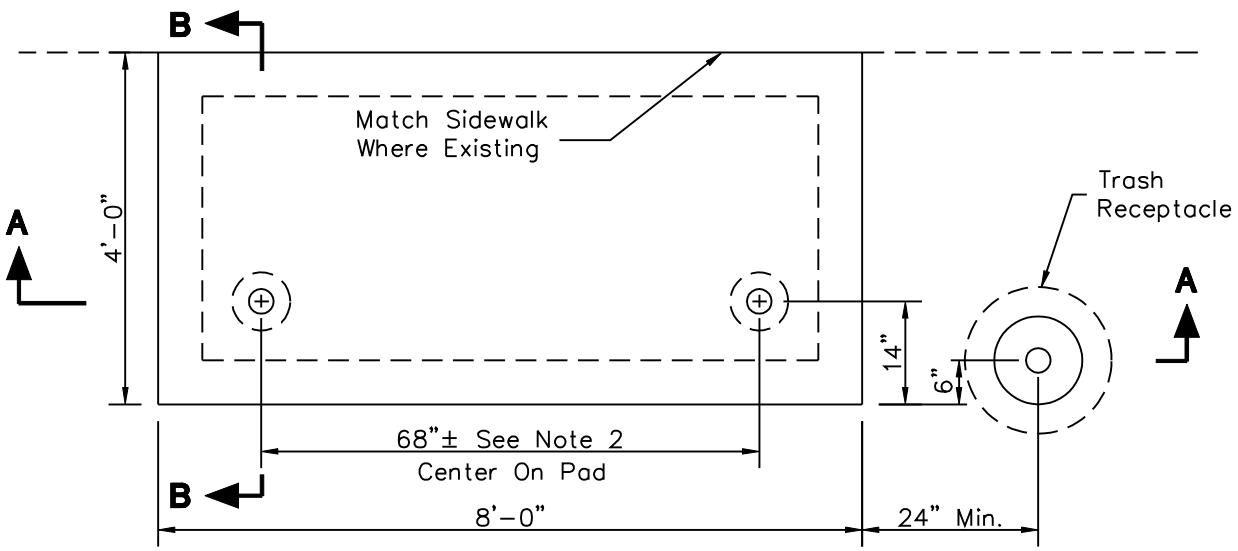




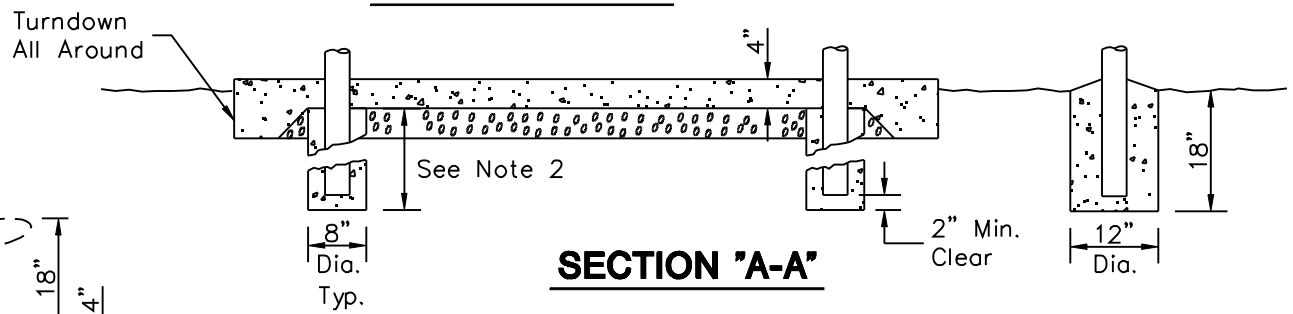
REVISED 5/17/06

DETAIL NO. 2267	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	FAR - SIDE BUS BAY	DETAIL NO. 2267

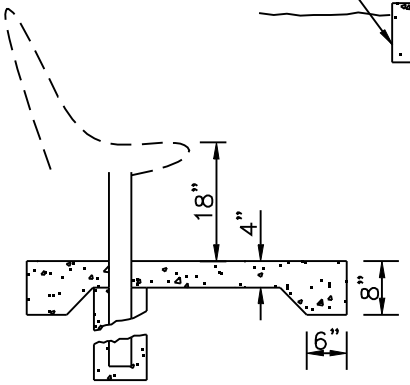
REVISED 7/1/97



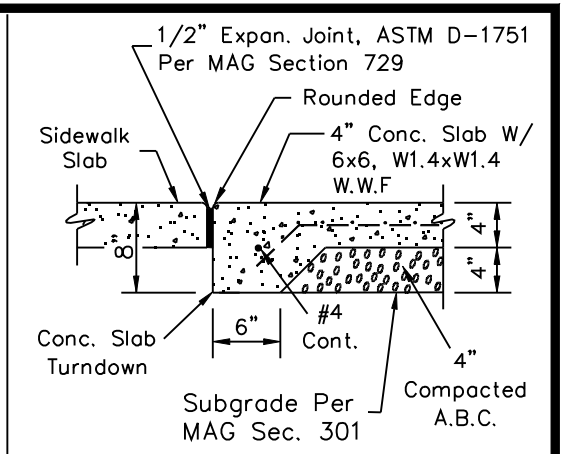
BASE SLAB PLAN



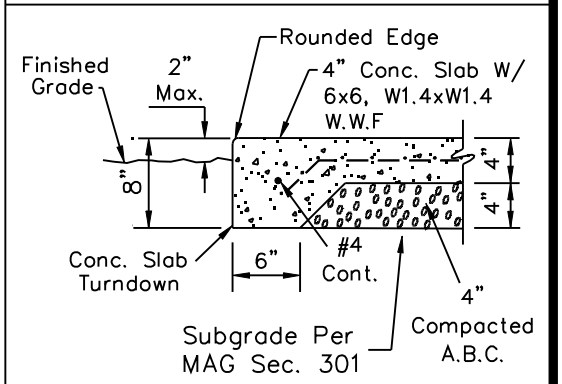
SECTION "A-A"



SECTION "B-B"



SLAB EDGE AT SIDEWALK



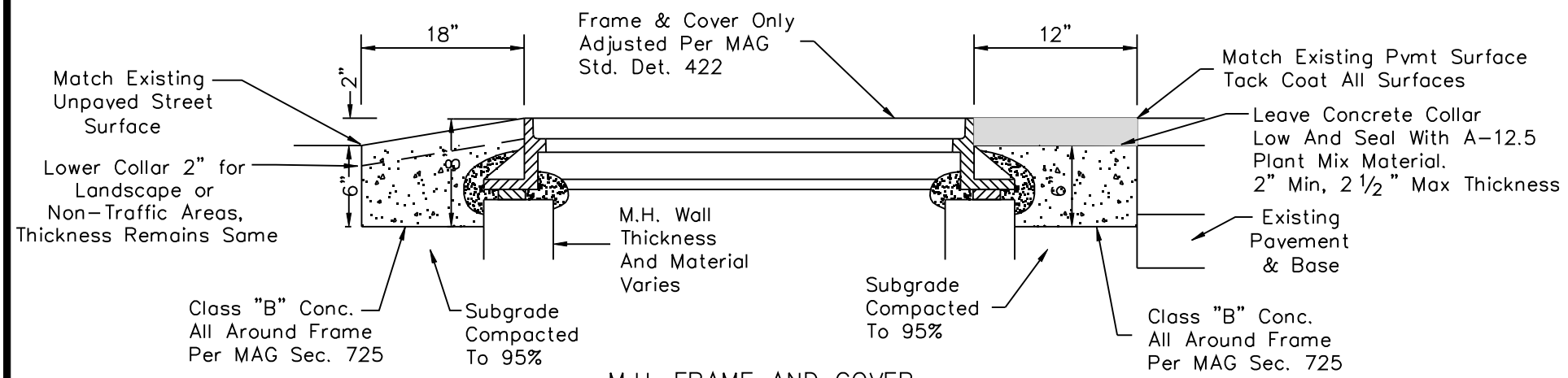
SLAB EDGE AT GRADE

NOTES:

1. Concrete shall be Class "A" per MAG Section 725. Slab surface shall be trowel finish with salt pitted appearance and random sweat. Use integral color when specified on the plans.
2. Dimensions may vary with bench style – Verify and adjust to provide clearance and bench height shown.

DETAIL NO. 2268	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	BASE SLAB AND FOUNDATIONS FOR BUS STOP BENCH AND RECEPTACLES	DETAIL NO. 2268

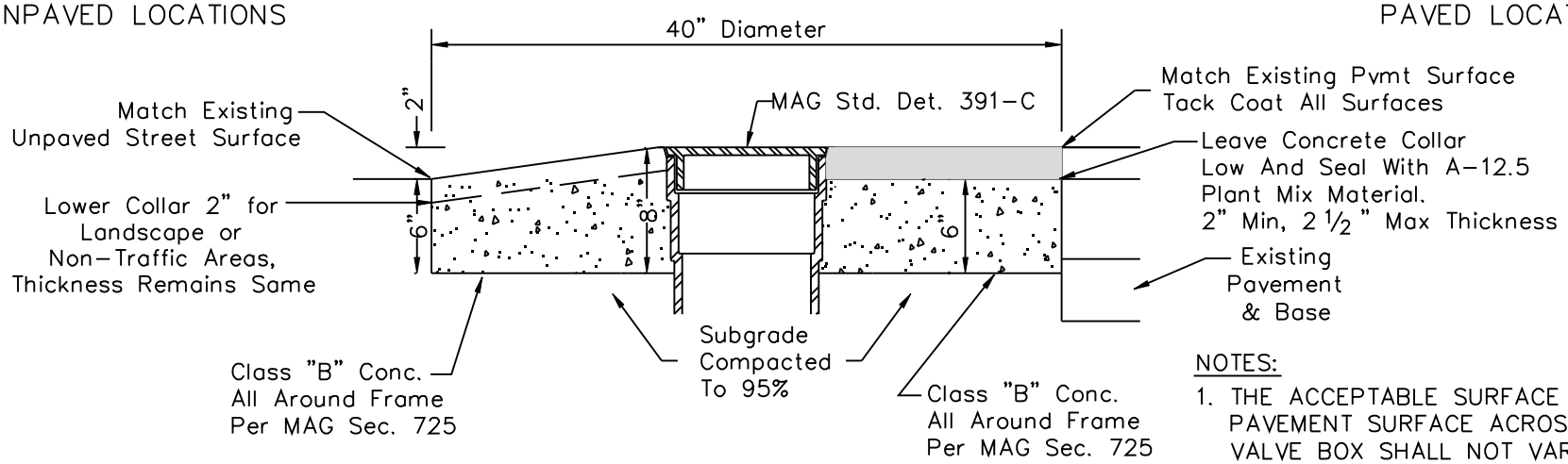
REVISED 2/27/01



M.H. FRAME AND COVER

UNPAVED LOCATIONS

PAVED LOCATIONS



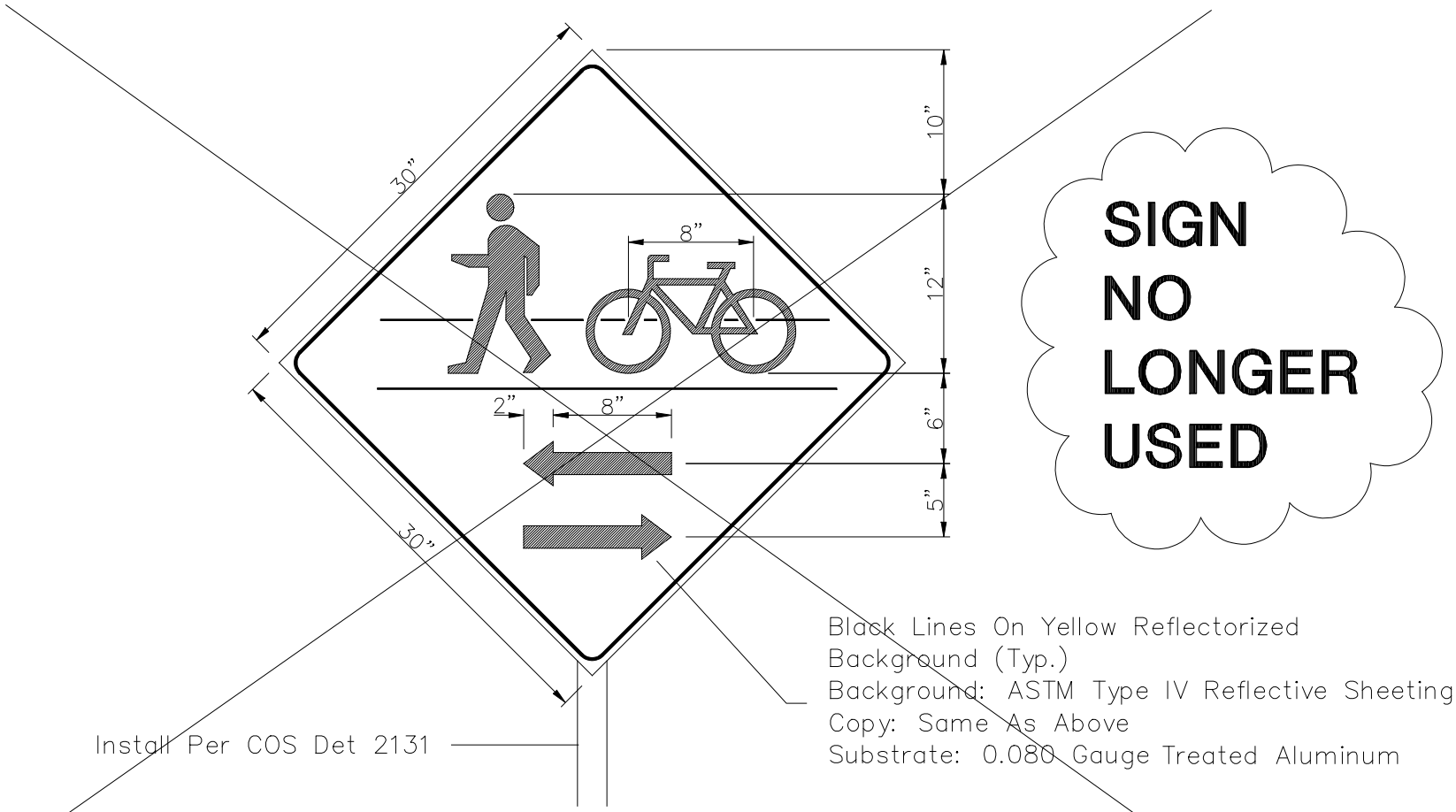
VALVE BOX FRAME AND COVER

NOTES:

1. THE ACCEPTABLE SURFACE PROFILE FROM THE PAVEMENT SURFACE ACROSS THE MANHOLE OR VALVE BOX SHALL NOT VARY MORE THAN 1/4 INCH FROM THE LOWER EDGE OF A 12 FOOT STRAIGHTEDGE WHEN THE STRAIGHTEDGE IS PLACED PARALLEL AND PERPENDICULAR TO THE CENTERLINE OF THE ROADWAY.

DETAIL NO. 2270	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	FRAME & COVER GRADE ADJUSTMENT	DETAIL NO. 2270

REVISED 4/24/07



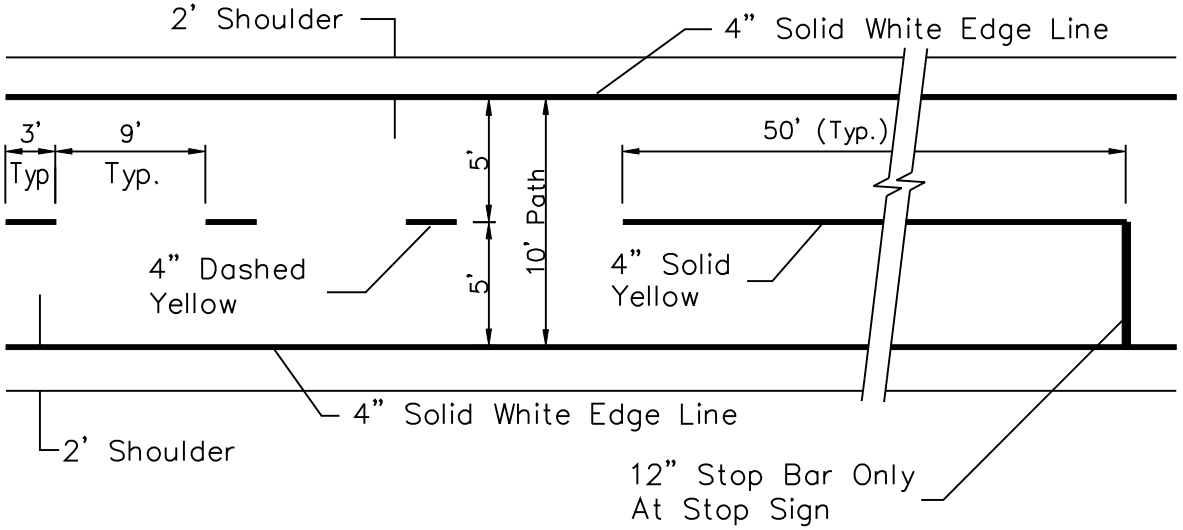
SIGN NO LONGER USED

Black Lines On Yellow Reflectorized Background (Typ.)
Background: ASTM Type IV Reflective Sheeting
Copy: Same As Above
Substrate: 0.080 Gauge Treated Aluminum

Install Per COS Det 2131

MULTI-USE PATH CROSSING SIGN

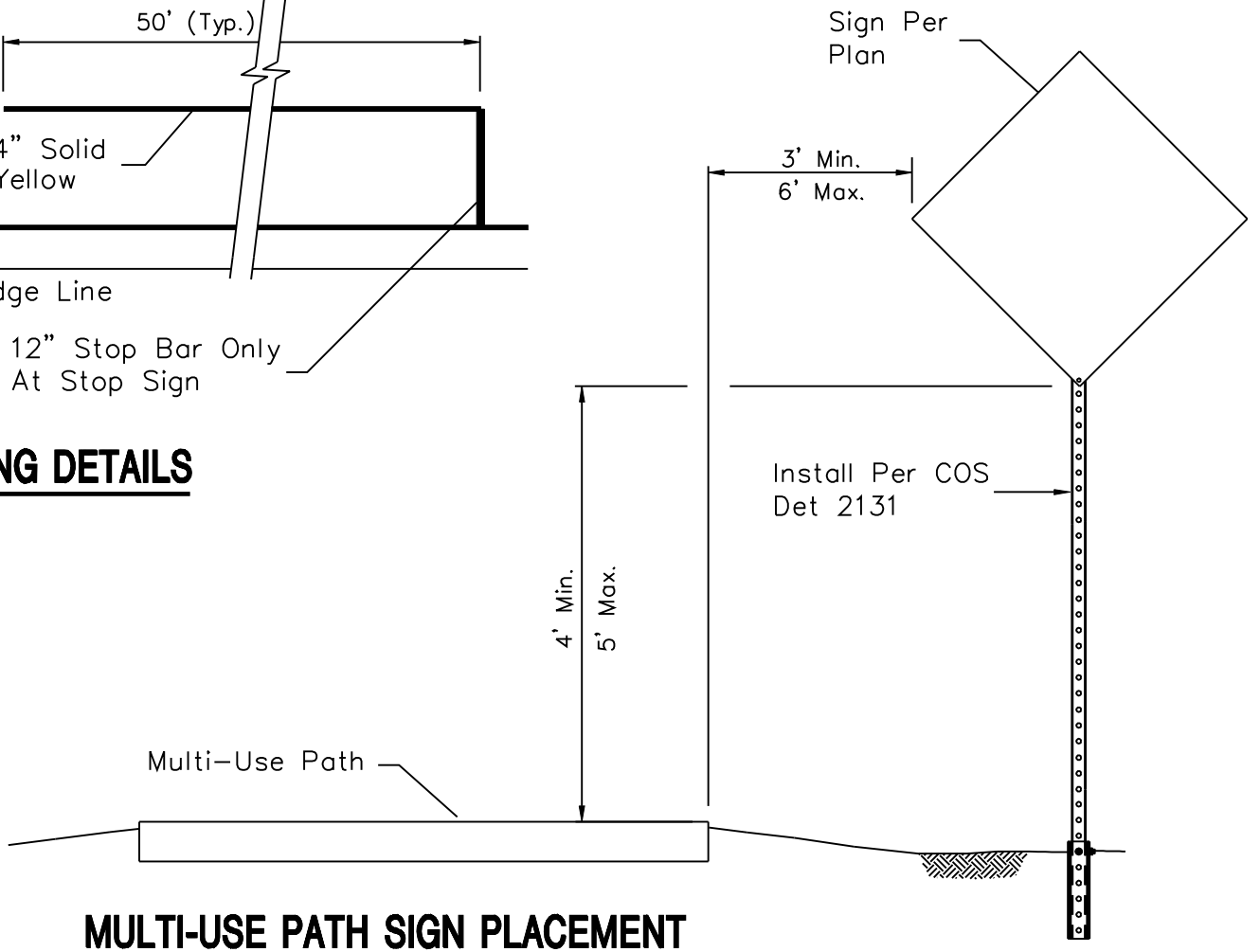
DETAIL NO. 2281	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	MULTI-USE PATH CROSSING SIGN	DETAIL NO. 2281



MULTI-USE PATH STRIPING DETAILS

NOTE:

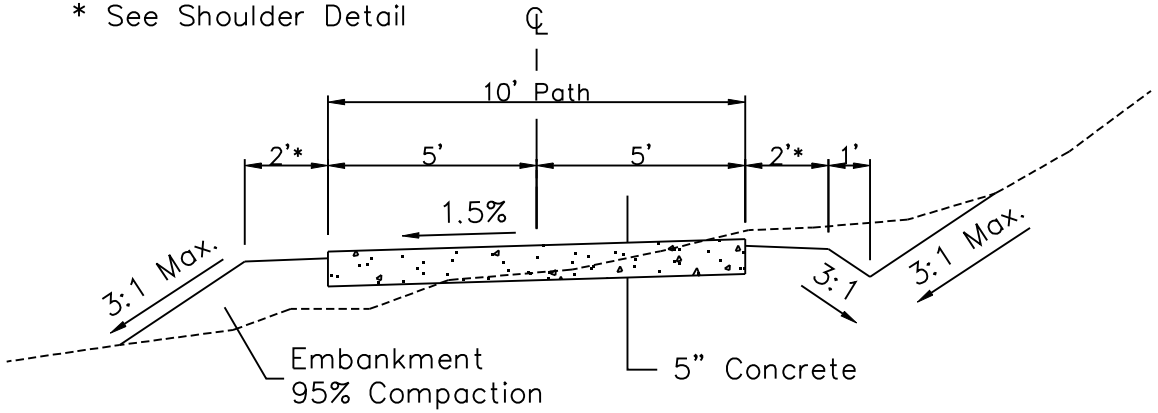
Striping shall be reflectorized paint with glass beads per COS Specifications Sec. 402.2



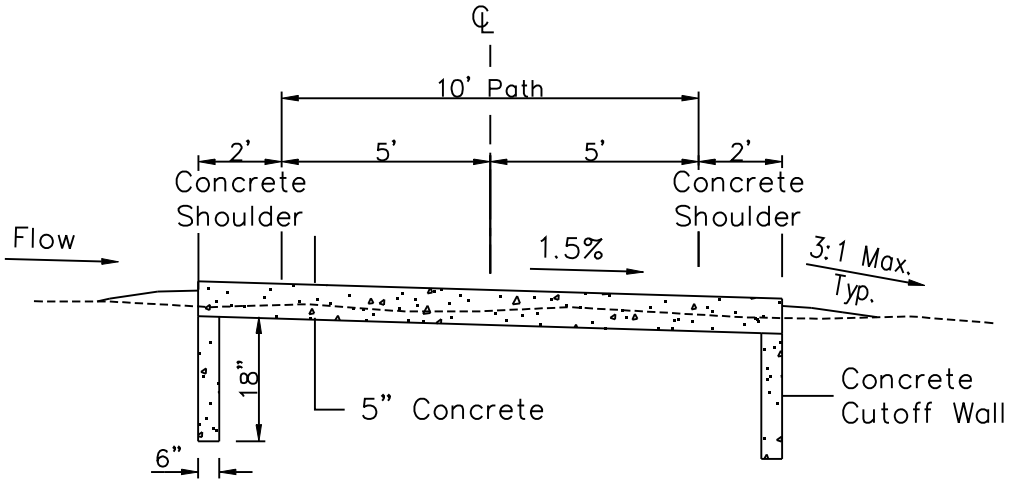
MULTI-USE PATH SIGN PLACEMENT

DETAIL NO.			
2282	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	MULTI-USE PATH STRIPING AND SIGNING — DETAIL NO. 2282

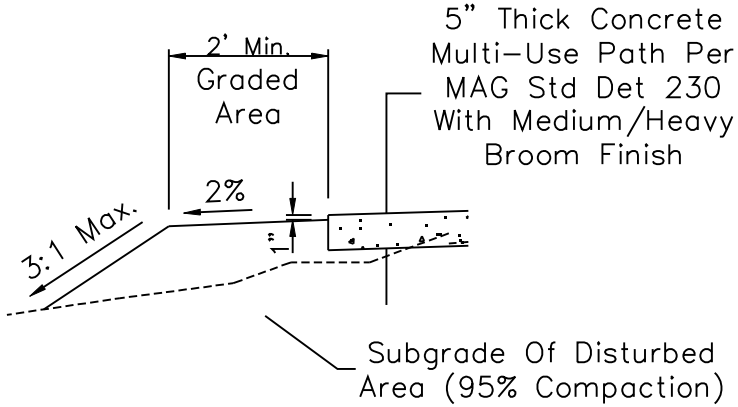
REVISED: 2/26/01



MULTI-USE PATH DETAIL



MULTI-USE PATH WET AREA CROSSING



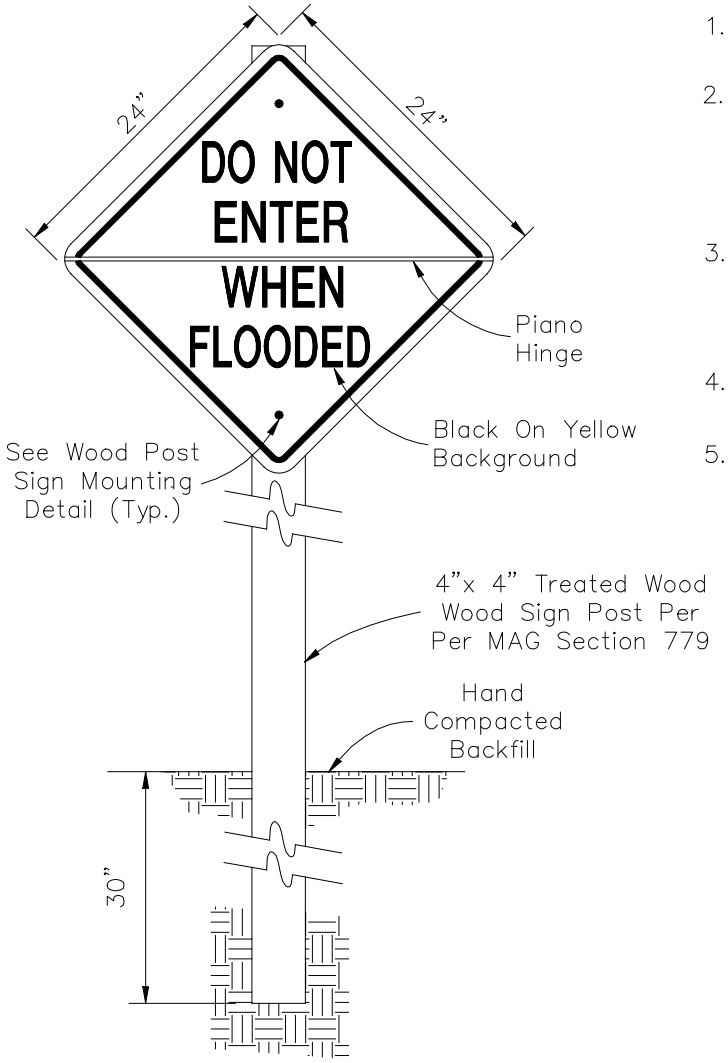
SHOULDER DETAIL

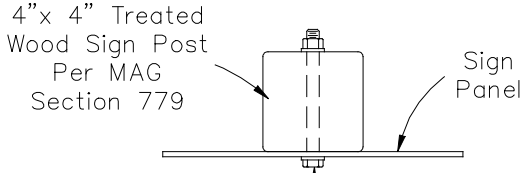
Note: A 3' x 3' Test Panel Shall Be Poured and the finish approved by COS Bicycle Staff At (480)312-7696.

DETAIL NO. 2283	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	MULTI-USE PATH DETAILS	DETAIL NO. 2283

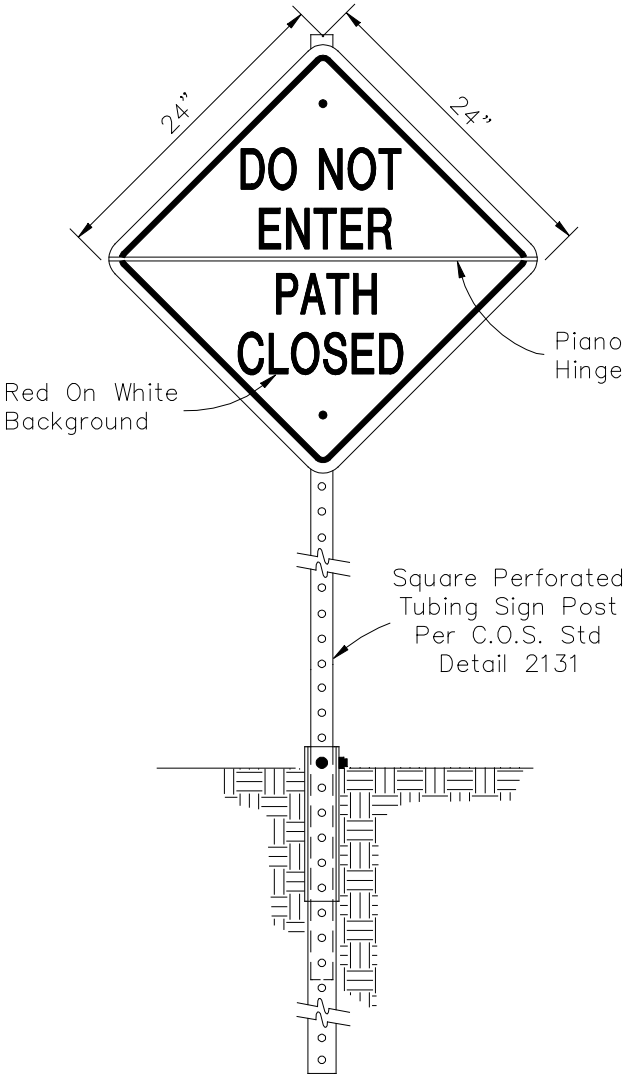
NOTES:

1. Signs shall conform to C.O.S. Supplemental Specifications, Section 402.3.
2. Signs to be mounted on square perforated tubing per C.O.S. Std Det 2131 within C.O.S. Right–of–Way. Treated wood post may be used for trail markers located outside C.O.S. Right–of Way.
3. Background and legends shall be ASTM Type IV reflective sheeting. Black legends shall be opaque (colors as noted) unless otherwise approved by the City of Scottsdale.
4. Sign height and placement shall conform to C.O.S. Std. Det. 2282.
5. For additional information regarding sign fabrication, contact the City of Scottsdale Sign Shop, 480–312–5646.





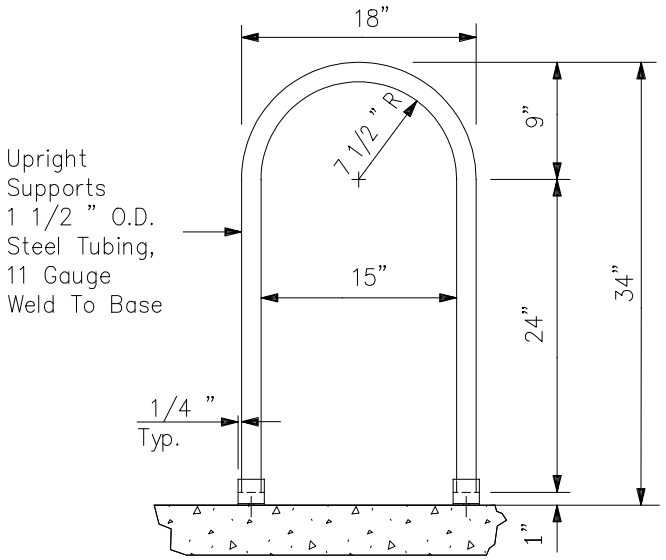
Fasten With (2) 1/2"x 2" Lag Screws With (2) Flat Washers or (2) 1/2"x 4 1/2" Bolts With (4) Flat Washers. Burr Threads And Nuts With Center Punch.

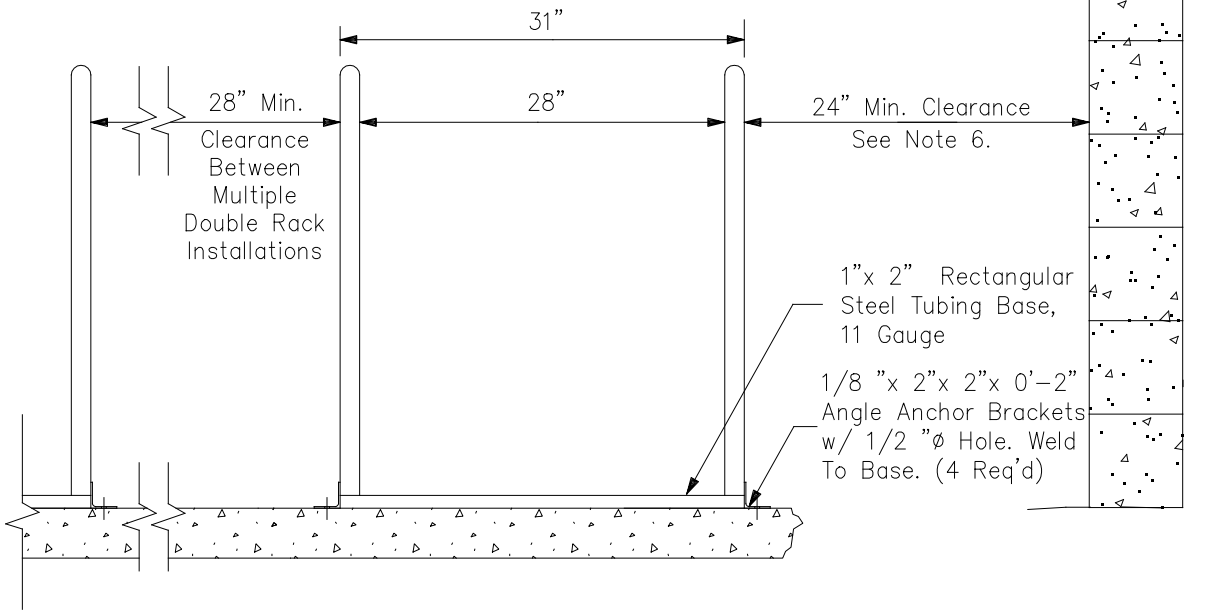


WOOD POST SIGN MOUNTING DETAIL

REVISED 4/24/07

DETAIL NO. 2284	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	MULTI-USE PATH WET CROSSING SIGN	DETAIL NO. 2284

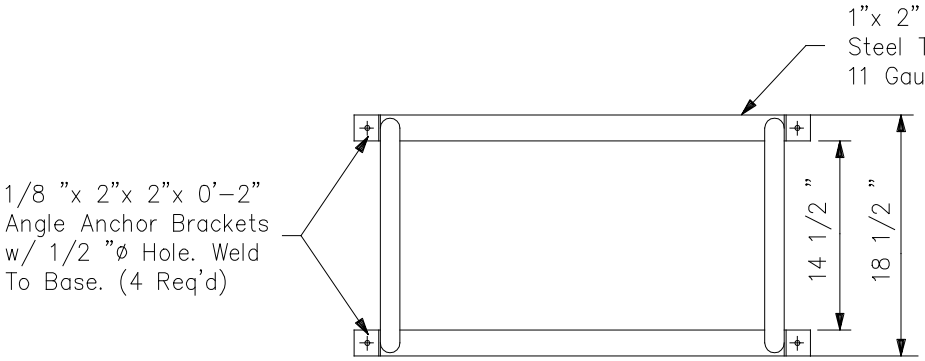




REVISED 5/09/07

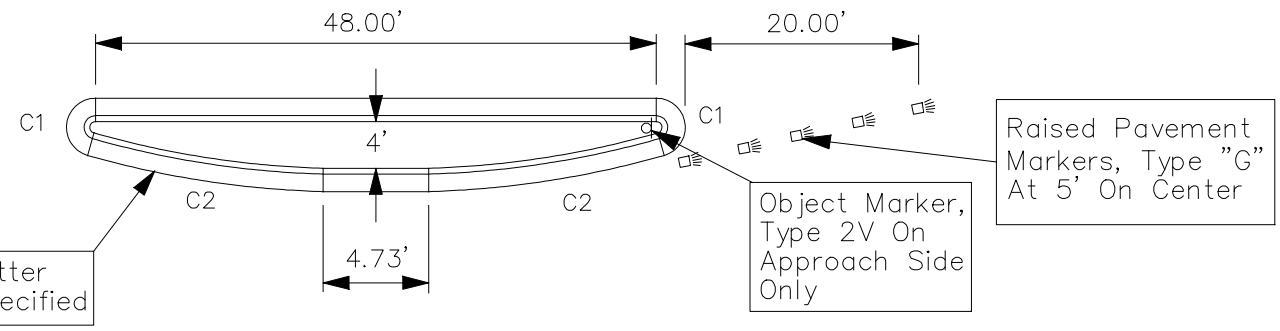
NOTES:

1. Double rack holds 4 bicycles.
2. Finish to be weather resistant, baked-on powder polymer coating.
3. Anchor rack to concrete w/ 3/8"ø x 2 1/2" wedge anchors and tamper resistant or welded nuts, (4 Req'd) or set tubing 12" below grade in 24" deep x 6" wide concrete footing.
4. Concrete base may be covered with turf or decomposed granite.
5. Placement of bicycle rack shall be convenient to main entrance and in a highly visible area.
6. 24" Min. clearance from walls or obstructions including curbs or edge of roadway on both sides and back of rack. Front of rack shall have a 6' min clear area.

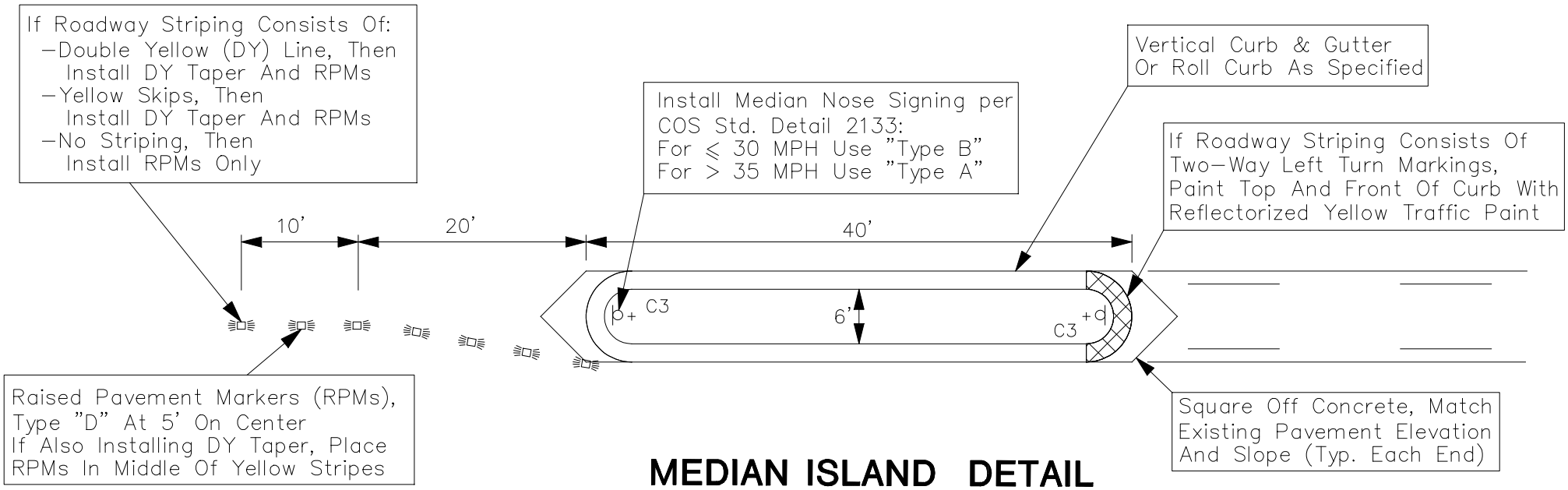


DETAIL NO. 2285	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	DOUBLE BICYCLE RACK	DETAIL NO. 2285

CURVE DATA – TO BACK OF CURB			
CURVE	RADIUS	LENGTH	DELTA
C1	0.50'	1.43'	164°12'35"
C2	80.00'	22.05'	15°47'25"
C3	3.00'	9.42'	180°00'00"



SIDE ISLAND DETAIL

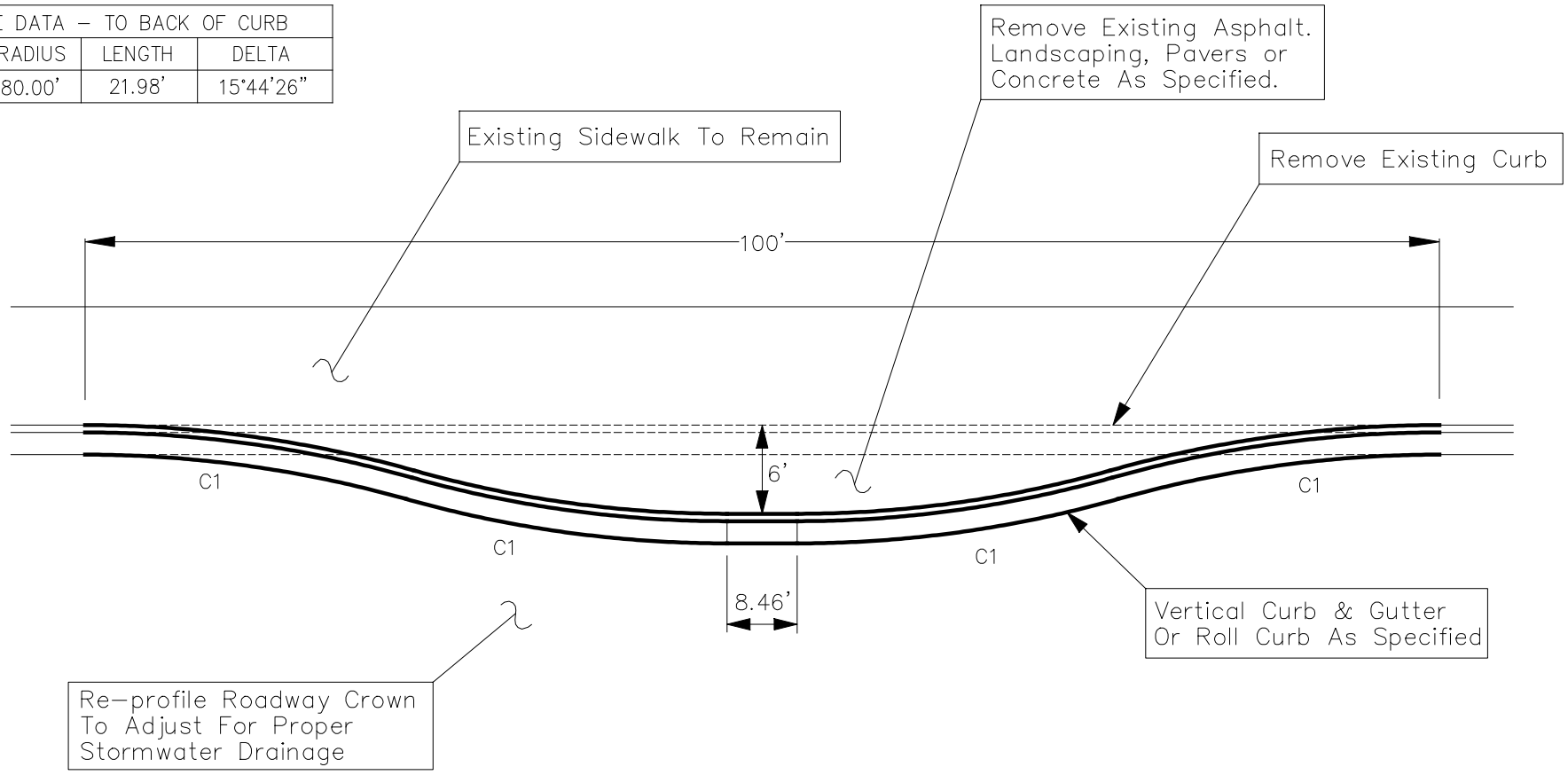


MEDIAN ISLAND DETAIL

REVISED 05/01/07

DETAIL NO. 2290-1	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	MEDIAN ISLAND DETAILS	DETAIL NO. 2290-1

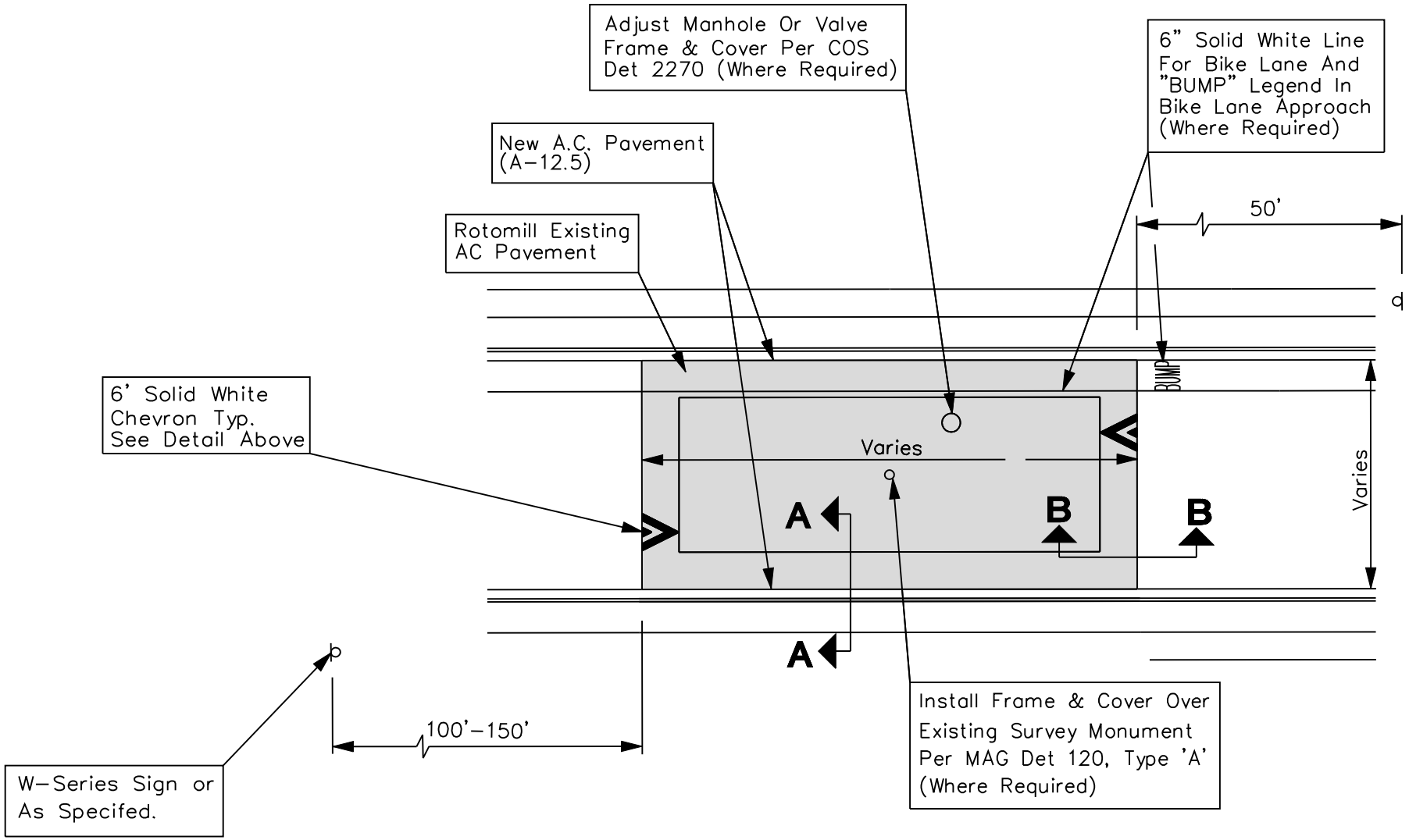
CURVE DATA – TO BACK OF CURB			
CURVE	RADIUS	LENGTH	DELTA
C1	80.00'	21.98'	15°44'26"



BULB OUT / CHOKER DETAIL

REVISED 05/01/07

DETAIL NO. 2290-2	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	BULB OUT/CHOKER DETAIL	DETAIL NO. 2290-2



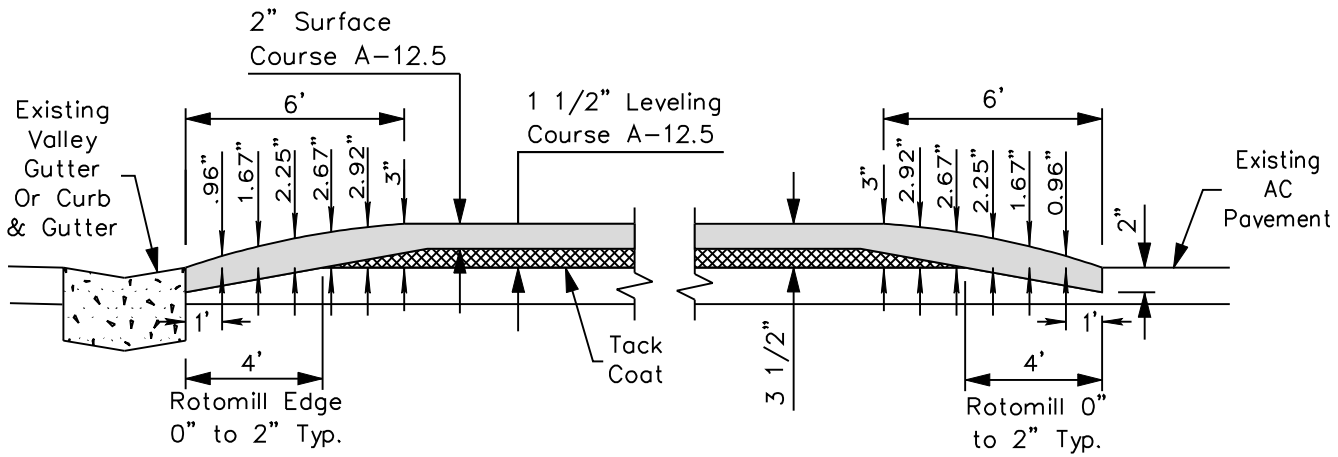
REVISED 5/4/05

DETAIL NO. 2292-1	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	SPEED TABLE DETAILS	DETAIL NO. 2292-1

REVISED 5/4/05

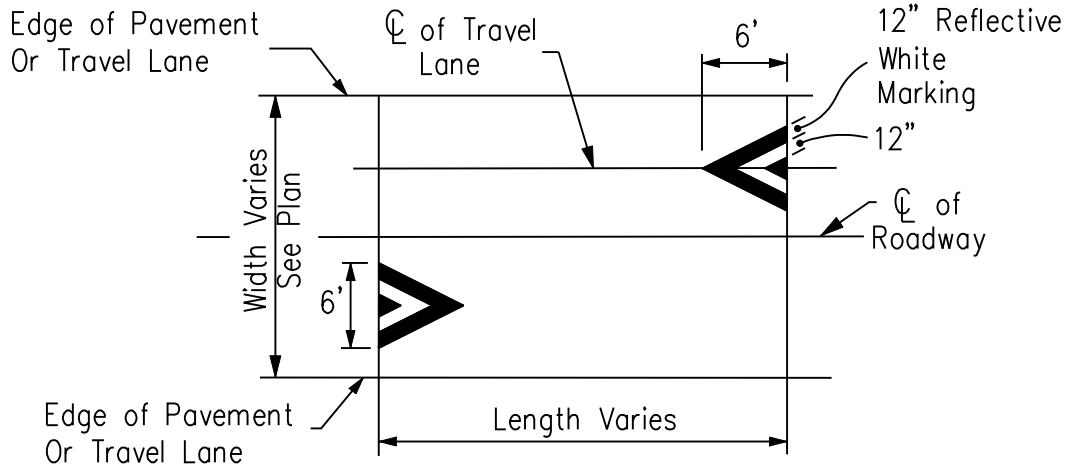
Warning Sign
30" x 30"

W13-1
18" x 18"



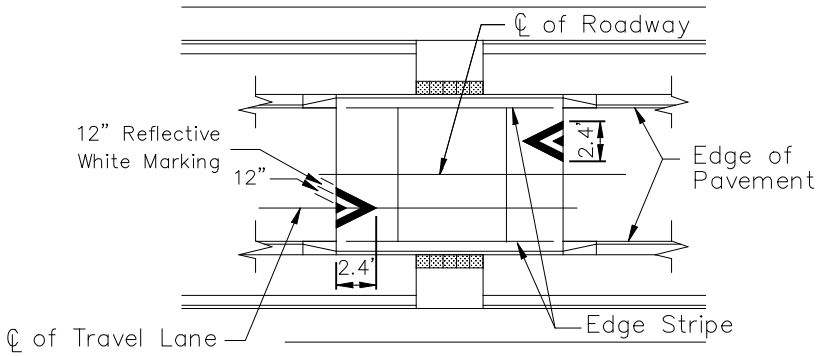
SECTION A-A
MATCH EX. CONCRETE
TYPICAL ALL LOCATIONS

SECTION B-B
MATCH EX. AC PAVEMENT
TYPICAL ALL LOCATIONS

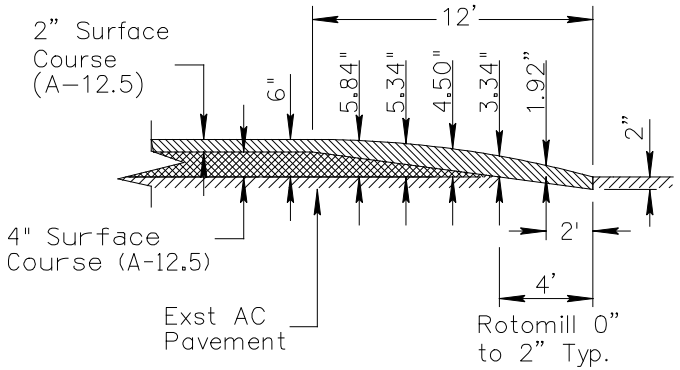


CHEVRON DETAIL

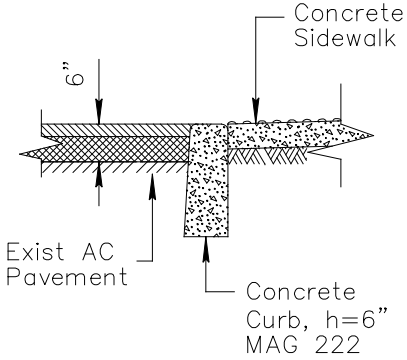
DETAIL NO.				DETAIL NO.
2292-2	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	SPEED TABLE DETAILS	2292-2



Chevron Detail

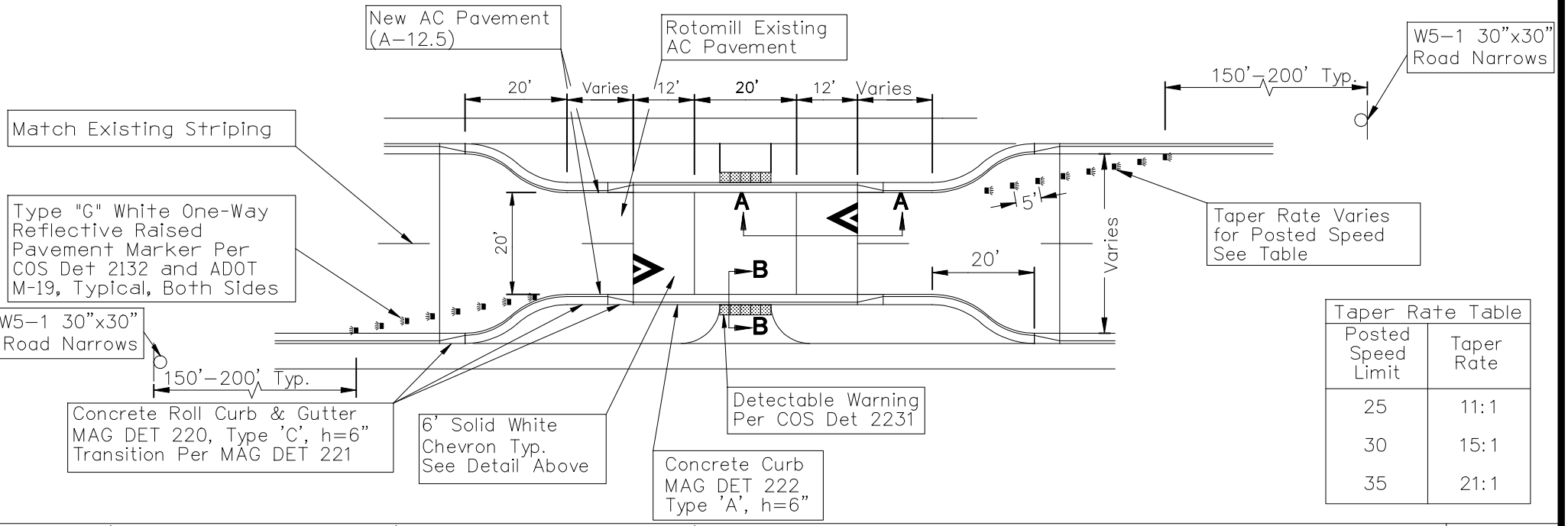


Section A-A



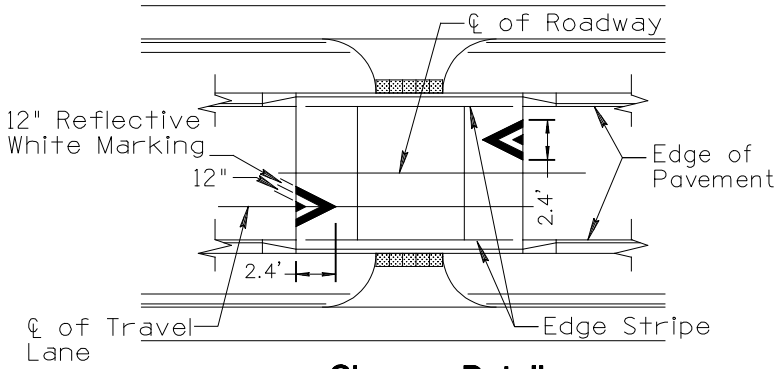
Section B-B

REVISED 5/25/07

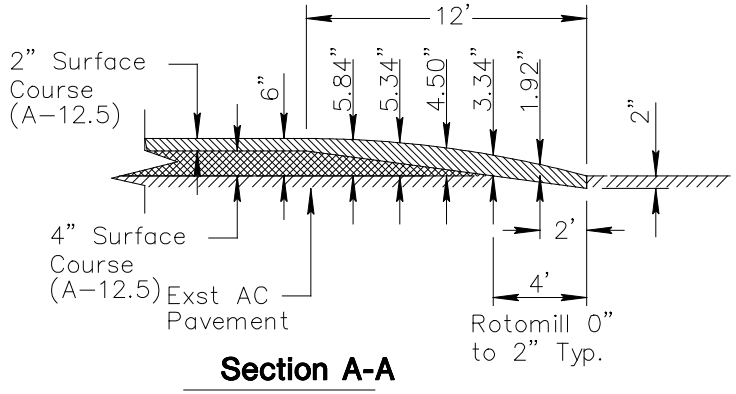


Taper Rate Table	
Posted Speed Limit	Taper Rate
25	11:1
30	15:1
35	21:1

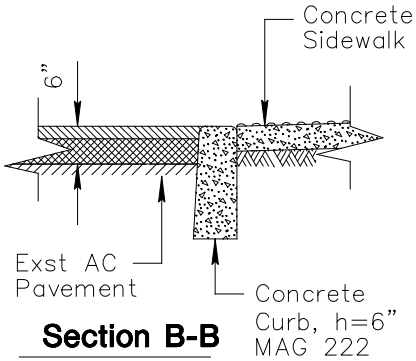
DETAIL NO.				DETAIL NO.
2293	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	MID-BLOCK PEDESTRIAN TABLE	2293



Chevron Detail

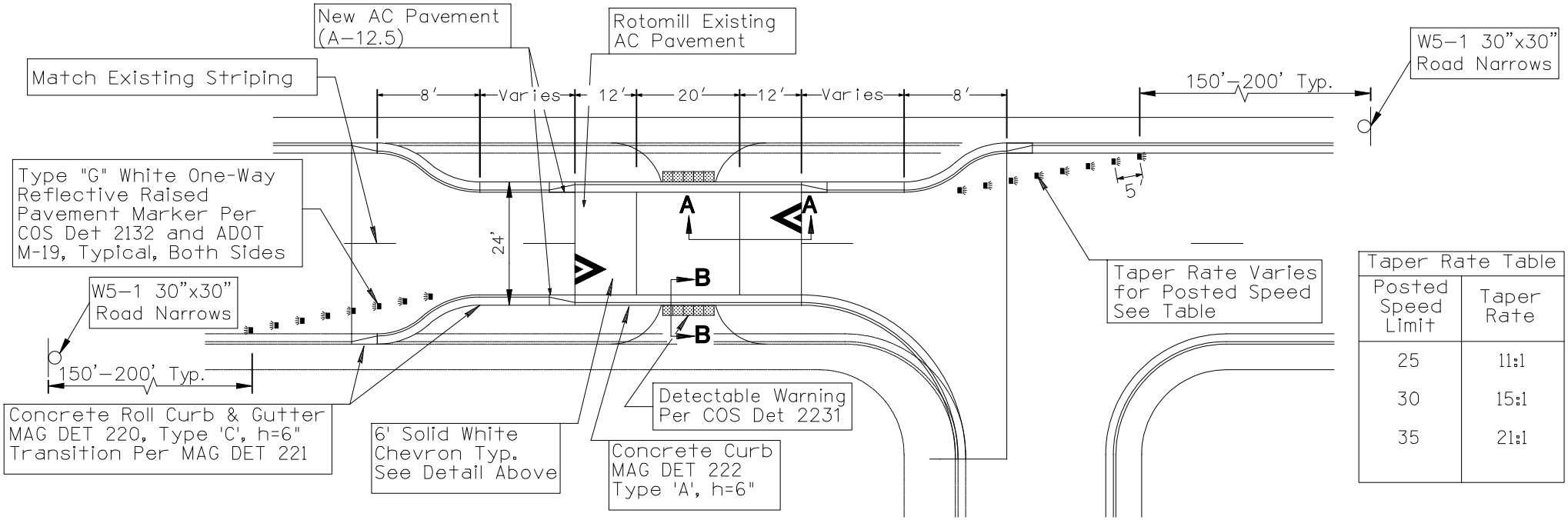


Section A-A



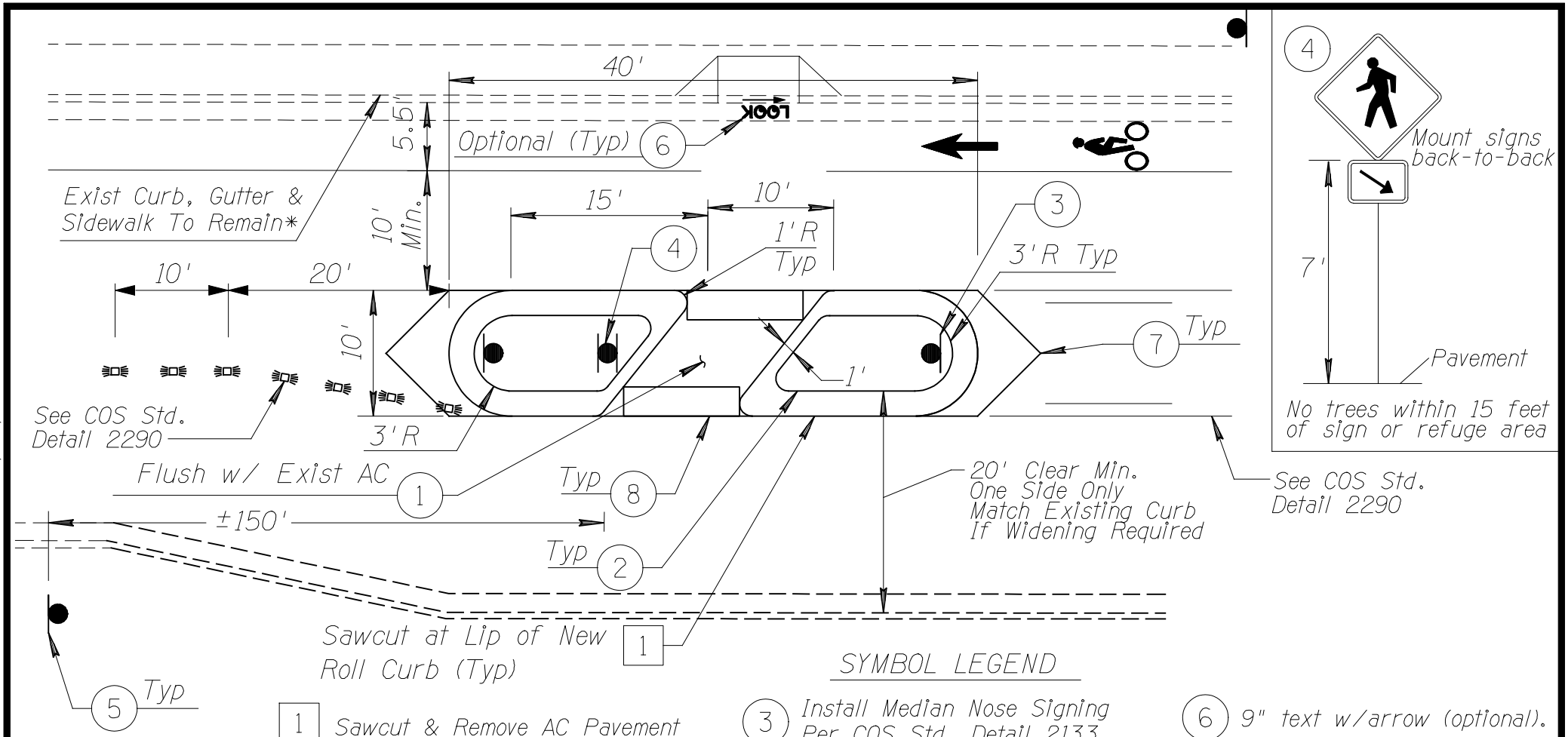
Section B-B

REVISED 5/25/07



Taper Rate Table	
Posted Speed Limit	Taper Rate
25	11:1
30	15:1
35	21:1

DETAIL NO. 2294	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	INTERSECTION PEDESTRIAN TABLE	DETAIL NO. 2294



REVISED 5/24/07

SYMBOL LEGEND

- [1] Sawcut & Remove AC Pavement
- (1) 8" Class "A" Concrete Pvmt per MAG Spec 324 & 725
- (2) Roll Curb per MAG Det 220 Type D
- (3) Install Median Nose Signing Per COS Std. Detail 2133
- (4) 30" x 30" W11-2 above 24" x 12" W16-7p-R below
- (5) If posted speed ≥ 40 mph. advance 30" x 30" W11-2
- (6) 9" text w/arrow (optional).
- (7) Square off concrete, match existing pavement elevation and slope.
- (8) Detectable Warning Surface per COS Det. 2231.

DETAIL NO. 2295	City of Scottsdale Standard Details	APPROVED BY: Scottsdale Standards & Specifications Committee	**PEDESTRIAN REFUGE**	DETAIL NO. 2295